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THE STRAND
RESTORATION & REVITALIZATION

A Study Prepared For
The Galveston Historical Foundation

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I. INTRODUCTION: THE STRAND

WHY SAVE IT?

The Strand of Galveston, once "the Wall Street of the Southwest", contains one of the finest concentrations of 19th Century commercial buildings in the United States. It is listed as an historical district on the National Register of Historic Places; it has been the subject of an Historic American Building's Survey; and it is dotted with historical medallions of the Texas Historical Commission. 7

Edmund Bacon, noted Philadelphia planner, called The Strand "the finest concentration of 19th Century commercial structures I have ever seen". Arthur Ziegler, a leader in urban preservation, wrote of The Strand:

"I think that it is one of the finest streets of its kind in the United States; indeed I cannot think of its equal. Its rarity derives not only from the fine quality and scale of the buildings but also from the fact that for the most part they have not been mutilated at street level".

The Strand stretches snugly between the central business district of Galveston to the south and the Galveston wharves to the north. It is within a block or two of the major financial and business institutions of the city. The wharf area contains shrimp fleets and party boats, holding the potential for a Fisherman's Wharf adjacent to The Strand. Only seven blocks to the east is the multi-million dollar complex of the University of Texas Medical Branch.

The attraction of the Strand buildings, their key location in the city, and their generally fine condition holds great potential for beautiful residential spaces and for a major tourist area. As townhouses and apartments the Strand buildings offer natural brick walls, 14-foot ceilings, tall vertical windows and rich-grained floors. Large open areas within the structures offer opportunities for skylights and interior atriums with plants and foliage.

Restaurants and retail shops can utilize similarly the unique potentials of the Strand buildings. Restored exteriors and beautiful interiors will be strong attractions to the thousands of tourists. As Galveston offers varied, interesting attractions such as The Strand more tourists will come, more will stay longer, and more will return again and again. The attraction of The Strand would be a fine complement to that of a Fisherman's Wharf complex, just a block away. With tourists able to enjoy fresh caught seafood amidst the bobbing shrimp boats and to tour shops and restaurants in the magnificent 19th Century blocks of buildings, there will be no similar attraction in the Southwestern United States.

The tourist and residential potential of The Strand is multiplied further by the cultural facilities already located there. The Galveston Arts Center at Kempner and Strand provides year-round classes in ceramics, photography, silkscreening and painting, to mention a few. Odetta taught guitar for a week; Mario Cooper, National Academy member, taught watercolor; and the Robert Joffrey Ballet Troup held a two week workshop. New York sculptor Harvey Bott and several prominent painters are refurbishing parts of Strand buildings for studio/living areas. Activities such as the Artworks Gallery, Island Inc's photography center, the Strand Surplus Senter, Estrada's wine imports, Georgette's Cafe Torrefie, and the Old Strand Emporium make The Strand a varied, interesting place to be.

Special events and programs continue to draw hundreds of people to The Strand. Last August the Arts Council's "Festival on The Strand" drew celebrities and artists from throughout the State and this February "Evening on The Strand" drew hundreds of people to view a renovated atelier-residence. This August 23, 24, and 25, the third annual festival will become Festival USA on The Strand. Talents from several cultural heritages and in each art form will come from Alabama, Mississippi, Louisiana, and Texas. 10,000 visitors are expected.

Finally, the possibility of renewed passenger train service between Galveston and Houston adds a potential windfall for The Strand. A feasibility study of such service will be completed shortly. Trains would arrive and depart from the Santa Fe terminal at 25th and Strand. Visitors could tour The Strand on foot, while using shuttle transportation to visit the Downtown, historical areas such as Ashton Villa, the Bishop's Palace, and the historical residential section, and the waterfront.

THE STRAND: HOW TO SAVE IT?

The goals of the revolving fund of the Galveston Historical Foundation grow directly out of The Strand's value and potential. The first goal is to preserve the historic buildings and restore their facades. The second goal is the active utilization of these buildings for the benefit of Galveston as a whole.

To these ends, the Galveston Historical Foundation, through its revolving fund, purchases Strand properties and resells them to private purchasers. In the case of an historic building the purchaser contracts to restore the facade within an agreed upon time, and to accept deed restrictions requiring GHF approval prior to demolition or changes in the facade. In the case of a non-historic building the purchaser may demolish the building at will, but he accepts deed restrictions requiring GHF approval of exterior design plans for new construction.

The immediate effort to restore and revitalize The Strand was initiated by the Galveston County Cultural Arts Council. In 1972, the Arts Council established the Arts Center on The Strand for the purposes of offering professional classes and workshops in the performing and visual arts and of focusing attention upon this important area. In February, 1973, the National Endowment for the Arts, in recognition of The Strand's potential, awarded the Arts Council an \$8,000.00 matching grant for nationally-known authorities to appraise The Strand and advise on a strategy for restoring and revitalizing it. Based on this advice, the Arts Council assisted the Historical Foundation in obtaining a \$200,000.00 grant from The Moody Foundation and a \$15,000.00 grant from the Harris and Eliza Kempner Fund to establish the preservation revolving fund.

The revolving fund grants were conditioned, however, on the Historical Foundation developing the administrative and financial capability to operate the fund effectively. For that reason, the Historical Foundation contracted in April, 1973, with the Washington, D. C. firm of Boasberg, Hewes, Klores and Kass to provide one of their attorneys, Peter H. Brink, as interim director for three and a half months. Subsequently, Mr. Brink took a leave of absence from his firm and accepted the position of Executive Director of the Historical Foundation.

Since that time, the following progress has been made:

1. The Historical Foundation has been restructured, new by-laws adopted, administrative offices established, financial controls developed, and broader community participation in the organization achieved, especially by the financial leaders of Galveston. New officers include: Dr. M. L. Ross, recently Mayor of Galveston, as President; Gene D. Wyatt, President of Moody National Bank, as Chairman of the Revolving Fund; and E. R. Thompson, Jr., Vice-President of U.S. National Bank, as Treasurer. Mrs. Edward F. Fugger, Jr., Chairman of the Galveston County Historical Survey Committee, is a member of the Steering Committee, along with Dr. E. Burke Evans, Mrs. A. D. Laskell, Mrs. John M. Wallace, and Mrs. Elbert Whorton. Emily Whiteside, Executive Director of the Arts Council, is a special advisor to the Steering Committee.

2. Special financing was obtained from Galveston banks to enable the Historical Foundation to purchase Strand buildings.

3. Six Strand properties were acquired by the Fund. Prices were set, and deed restrictions and contractual obligations drafted.

4. The six properties were placed on the market through a cooperative effort by fourteen Galveston Realtors. Within two weeks the first sale (the James Fadden Building) was made with exterior restoration already begun. The purchaser, Swain Restoration, Inc., plans apartments on the second floor and commercial space on the ground floor. Since then two additional properties have been sold.

5. Agreement has been reached with the leading financial institutions of Galveston to provide \$1,000,000.00 in financing on especially favorable terms for purchase and development of revolving fund properties by private investors.

6. Negotiations are underway for the purchase of facade easements on two key Strand buildings not otherwise available for purchase. Approaches have also been made for the donation to the revolving fund of two other Strand buildings.

7. An additional Strand building has been purchased by Emily Whiteside, who has commenced exterior restoration and development of three sophisticated townhouses in the building. Part of another building is now being adapted by Bill Fullen as a wine and cheese emporium, and renovation efforts in at least two other buildings are continuing. Other Strand owners are painting their buildings as the momentum of the Strand project grows.

In addition, the Galveston County Cultural Arts Council continues its efforts to develop adaptive use for the Strand buildings, such as the Artworks Gallery, and to provide programs on the Strand, such as "Festival on the Strand", to help in its revitalization. The Arts Council is also spearheading the organizing of the Bicentennial in Galveston, and revitalization of The Strand is a major goal of Galveston's Bicentennial Celebration.

THE STRAND: PLANNING

At the same time the above steps were being taken, the Historical Foundation moved forward with planning for the Strand area. Presentations were made to the City Planning Commission and the City Council, and the support of both obtained. Proposals were submitted to the city for special revenue sharing funds for capital improvements on The Strand and to the National Endowment for the Arts for planning funds under the City Options Program. Subsequently, a \$42,000 grant has been awarded to GHF to develop a master plan for the linkages of The Strand to the Downtown, to the waterfront wharfage area, to the University of Texas Medical Branch, to Ashton Villa, to other residential historic areas, and to the beachfront.

Planning for The Strand has also received a solid boost with the commitment of the School of Architecture, University of Texas at Austin, to devote four consecutive semesters of class time over a two-year period to adaptive use designs. The University of Houston School of Architecture is also involved in design study.

The Historical Foundation engaged the firm of Ford, Powell and Carson, leaders in restoration efforts in San Antonio, to develop the exterior ambiance study of The Strand. The study first discusses

one major issues to be dealt with in the planning and development of The Strand. It sets forth basic principles to guide the visual development and the restoration work of the Strand effort. Finally, it makes detailed recommendations, with cost estimates, for overall physical improvements in The Strand. These recommendations deal with street lighting, landscaping and green spaces, signage, sidewalks, curbs and street and walk paving.

THE STRAND AND HISTORIC GALVESTON

The Strand, however, cannot be considered in isolation from the rest of Galveston nor without reference to the unique events which have allowed it to remain in existence. Galveston as a whole contains such extensive and rich historical areas that it must be ranked in the class of Savannah, Charleston, and Annapolis. Fine 19th Century Greek Revival and Victorian houses cover block upon block of the city, while the downtown area in and around The Strand contains scores of excellent 19th Century commercial, many iron-fronted, structures. In speaking of Galveston, Lee Adler, leader of the Savannah restoration, appraised the Island "as every bit as good as Savannah".

The architectural and cultural heritage of Galveston is uniquely significant because Galveston's history is unique. In the mid and late 1800's, Galveston was the financial and cultural center of Texas and the Southwest. Thousands of Americans and immigrants established themselves in the Great Port of Galveston, or passed through it on their way to new homes throughout the Southwest. Cotton from all Texas flowed out, and goods from Europe and New England flowed in. Commercial activity centered in The Strand, the grand street conceived from London's Strand and located only a block from Galveston's thriving wharves.

Galveston's heyday allowed wealthy Galvestonians to build the best in 19th century houses and commercial buildings, including two opera houses and several fine hotels. Prosperity filtering throughout the community produced hundreds of typical middle and lower income houses and cottages across a large portion of the island. Events of epic drama left their mark upon the structures: The Battle of Galveston in which the Confederacy broke the Union blockade of the port; the 1900 Storm in which 6,000 persons perished as tidal waves swept the island; and the consequent construction of the seawall, and the grade-raising in which much of the island was raised five and six feet by the pumping of fill by dredges.

In the early 1900's, however, Galveston lost its commercial pre-eminence to Houston, 45 miles to the north. Houston succeeded finally in constructing a ship channel allowing ocean-going ships to bypass the Port of Galveston, and the railroads decided to make Houston their major terminal in the area.

But this very commercial failure on Galveston's part made possible the survival of much of the city's 19th Century historical areas. Since that time a reduced level of commercial construction, and an absence of federal urban renewal, has left historic Galveston substantially intact.

Today, efforts to preserve and revitalize this network of historic structures stand at a crossroads. On one hand, deterioration in many 19th Century structures and short-sighted commercial activity are pushing the city toward the expedient course of arbitrary demolition and cheap, mass produced construction. Should Galveston follow that course it will, within a decade's time, become indistinguishable from similar-size cities throughout the United States.

On the other hand, Galvestonians are now rallying to a preservation and cultural renewal effort whose goal is the saving and revitalization of historic Galveston as a whole. Should this course be followed, Galveston will preserve and enhance its unique beauty and character so that visitors, appropriate industries, and prospective residents will be attracted to Galveston precisely because of these assets.

A COMPREHENSIVE PRESERVATION EFFORT

During the past several months the effort to restore and revitalize historic Galveston has made substantial progress. The central thrust of the effort is the Strand revolving fund described above. But moving forward with the Strand are three other major efforts of the Historical Foundation, each of which will strengthen, and be strengthened by, progress on The Strand. Briefly these efforts are:

1. The restoration of Ashton Villa, a regal antebellum mansion, located on the major boulevard of the city, to be used for public tours, civic and arts functions, private receptions, and a visitors' center. Ashton Villa will provide a setting for thousands of visitors to become acquainted with all of the historical and cultural attractions of Galveston and will be the Bicentennial welcoming center. To date over \$350,000.00 has been raised for acquisition, restoration and furnishing of the Villa, and over \$90,000.00 in period furniture donated. The restoration work has been inspected by Secretary of Interior, Rogers Morton and by James Biddle, President of the National Trust for Historic Preservation. The Villa opened to the public on July 27, 1974.
2. Completion of a comprehensive historical survey of all Galveston Island, utilizing survey work previously done by the Texas Historical Commission and the Historical American Building Survey. Emphasis will be on extension of the existing residential historical district established under the local zoning ordinance.

The survey, under the expert guidance of Ellen Beasley, will result in a publication whose main purpose will be to make Galvestonians and visitors fully aware of Galveston's unique historical areas. Discussions are now under way with the government, with indications that adequate HUD 701 funds will probably be made available to complete the survey.

3. An extensive program of community awareness and education. Part of this effort consists of activities for members of the Historical Foundation, including outside speakers, films, exhibits, and working field trips to Savannah and elsewhere. The 1840 Samuel May Williams House and St. Joseph's Church, both administered by the Historical Foundation, are key sites for many of these activities. Membership in the Historical Foundation now stands at over 1,200 members. Another major part of this effort consists of public events sponsored by GHF, by the Galveston County Cultural Arts Council and the Galveston County Historical Survey Committee. Galveston, due to the arts council, was selected as one of three cities in the nation for an ARBC Festival USA pilot project using the arts to create community awareness and resulted in performing and visual artists from five Texas cities joining Galveston artists and the business community in a day-long event entitled "Joy to the World". A Children's Bicentennial Museum and holiday heritage exhibit took place on The Strand. Historical exhibits sponsored by GHF were held city-wide by school students during Public Education Week.

Closely related to all of the above efforts is the program of the arts council to acquire, restore and program the 1894 Grand Opera House for use as a performing arts facility and the program, led by Mrs. Mary Moody Northen, for an outdoor historical drama to be performed in the State Park at west Galveston Island.

GALVESTON AND ITS FUTURE

Galvestonians have within their grasp the means to double or even triple their income from tourism; to attract research-oriented, clean industries; to provide superb residential accommodations on the island; and to enjoy a way of life the envy of most places in the United States. The means to these economic and personal benefits are the wise development of two of the island's major assets; first, the good beaches, sunny weather, and clean air; and second, the network of beautiful 19th Century residences and commercial buildings throughout the entire eastern portion of the Island.

For Galveston to reap these benefits she must move forcefully to protect and enhance these assets. For the city stands at a crossroads:

on the one side lies the quality development suggested above while on the other lies haphazard, short-term exploitation of the beachfront and piece-meal destruction of the rich 19th Century areas by arbitrary demolition and cheap, mass produced construction. Yet in the last few decades only a portion of the city's people and governing institutions have grasped the nettle. Too many others are attempting to be all things to all interests; to have both uncontrolled development and quality development; and to reach decisions by a lively balancing of short-term interests with insufficient consideration of the long-term consequences.

In recent months, however, more and more leaders and members of the community are joining in a revitalization effort the goal of which is the renaissance of Galveston as a uniquely beautiful counterpart to, rather than a poor imitation of, the Houston megapolis only an hour's drive to the north. This effort could not come at a more propitious time. In less than two years, the era of the American Revolutionary Bicentennial commences. Just as the period leading to July 4, 1776, was a time of great decisions so must these current years be the time for Galveston to place itself upon a path of long-term revitalization and quality development.

While this must clearly involve decisions in areas ranging from health and education to traffic control and zoning, the Galveston Historical Foundation is by nature concerned foremost with the preservation and wise utilization of the fine historical structures throughout the city. For this reason, this introduction has focused primarily upon the significance of this historical architectural asset and the progress being made in developing this asset. Such discussion is provided as a basis for consideration of the recommendations contained herein for overall physical improvements in The Strand area. It is hoped that this introduction will make clear that, while these specific improvements are relatively narrow in scope, they represent a crucial step forward in the preservation and enhancement of historic Galveston so vital to the economic well-being and individual ways of life of Galvestonians.

II. Development Concept for the Strand and Environs

The development concept for The Strand and its environs is concerned with the procedures necessary to implement the goal of restoration/preservation considered in the introduction of this study. Two issues which are primary concerns in this development concept are (1) economic feasibility and (2) local administration and control.

1. Economic Feasibility: this issue is strongly influenced by the local financial institutions which will largely finance the restoration/preservation effort in the city. It is often difficult to secure loans on projects in areas such as The Strand or for preservation projects in general since many are considered high risk ventures. Obviously a substantial amount of such money must be available if historic Galveston is to be saved. Two basic alternatives are available for local financing or restoration/preservation. One is for the financial institutions to consider projects on an individual basis, taking care to divide the number of projects equitably among participating institutions. This is the simplest, and perhaps best way to proceed. This method, however, has the inherent problem of individual projects which might not be funded because of the particular risk involved. Each lender will logically want the most secure projects with the least amount of risk.

A second alternative is to arrange financing on a participation basis. Essentially, the lending institutions would join together to provide loan funds, with each participant accepting a portion of the total risk. Loans might be made through a loan committee with sufficient authorization from the participants. This participation financing has several advantages. The lenders equitably share the risk. The loan committee would understand the total development concept as seen by the Galveston Historical Foundation and the city. Projects of greater

risk, but critical to the fabric of the Strand area would have a greater opportunity to be funded. Basically this is a concept where local financial institutions join together to improve the local community with local money. The method has been used successfully in other cities with local non-profit housing and community development corporations. Houston, San Antonio, and Austin institutions have used participation financing for local non-profit development corporations.

The American National Insurance Company presently provides low interest housing loans to employees if they locate in Galveston. This subsidy if applied to the Strand area, could encourage employees to utilize rehabilitated housing almost directly adjacent to their work. Other businesses and institutions, such as the University of Texas Medical Branch, should be encouraged to promote island housing policies which could similarly benefit the Strand.

Another issue directly related to economic feasibility is the property taxing system. In order to encourage restoration, owners who undertake to restore an historic area building could be allowed some flexibility in payment of property taxes. One possibility is to apply a graduated tax which increases over a reasonable period before the maximum new tax valuation is assessed. Another method would be to permit a tax increase moratorium to extend for several years after restoration. A provision to provide such flexibility to achieve public goals of this type is now included within the proposed revision of the State Constitution and will be considered by a Constitutional Convention in 1974.

2. Local Administration and Control: The Galveston Historical Foundation, as a primary instrument for restoration/preservation, will have control over the buildings it buys and sells with its revolving fund, and thus should have the strongest influence on the Strand development. In terms of long range planning, however, the city must become active in the restoration/preservation effort.

This might best be accomplished by creating a special division within the city planning department. This division would act to coordinate the application of the various city codes and ordinances which will affect the restoration/preservation effort. Without the proper support within city government, efforts of the Galveston Historical Foundation and others in preservation activities may be largely frustrated. Specific planning issues, in which city assistance is needed, are discussed in the following section.

III. Planning Issues and Opportunities

The degree of success of the current efforts on The Strand will be closely related to solutions to certain city wide problems. The Galveston Historical Foundation must act strongly to encourage a rational and creative approach to planning at all levels of government which have influence on The Strand. Additionally, business oriented organizations such as the Chamber of Commerce and public and private institutions often are involved in planning activities which could significantly effect the preservation restoration effort. The unification and coordination of planning and programing at these diverse levels is critical if The Strand area is to become again a viable part of the city's fabric.

Significant planning issues and opportunities for The Strand are beginning to come into focus. The following appear to be most important.

(1) Locational Factor

The Strand area is strategically located adjacent to downtown Galveston between it and the Galveston Wharves. The Wharves, with their attendant cotton warehouses and the small mosquito fleet, is one of the most historically interesting areas of the city. The Strand occupies a central place which could serve as a linkage between the waterfront and the downtown. At present, there are various threats to the port district. There is a proposal to place an expressway between The Strand and the waterfront. The proposal which would damage the waterfront character and interfere with reasonable pedestrain and vehicular access to the wharf area, would also damage The Strand, and must be resisted without compromise.

(2) Proposed Fisherman's Wharf

The mosquito fleet is an integral and most important part of the Wharf area. These shrimp boats operate presently out of a small harbor near the north end of 20th Street. There is a possibility that this fleet will be moved elsewhere on the island. This would remove from the Strand area one of the most delightful and characteristic economic activities along the Gulf Coast. It is very important to retain this activity at^{or near} its present location.

The waterfront area would be the ideal location for the much discussed Fisherman's Wharf with piers, boat service, restaurants and other people and water related activities.

There may be, in addition, a possibility of moving certain waterfront related warehouses that are in the vicinity of 20th Street. This area, owned by the Galveston Wharves, might provide a wonderful small open air place for a market and other cultural activities. One such use could be as an open air theatre for occasional performances.

(3) The Santa Fe Building

With the increasing interest nationwide in mass rapid transit, commuting, and inter-city trains such as Amtrack, it is likely that passenger trains will again run between Galveston and Houston as in the days of the Old Interurban. The pace toward that day is now being accelerated with the undertaking of feasibility study of such service. If service is restored, the logical place for a terminal in Galveston would be at the head of The Strand in or adjacent to the Santa Fe Bldg. This would be a tremendous economic boost to the Strand revitalization program. It would provide a very important nucleus of activity which would generate economic investment in the area. The arrival and departure of trains would add an element of drama. Perhaps no single opportunity possesses greater potential for invigorating The Strand and Galveston. Economic and cultural interchange with the Houston area and beyond could take place by an "arts train". It could be a

combination of museum, gallery, and stage, filled with the most recent art works from other places.

(4) Intra-City Transit and Parking

The most important economic generators of Galveston are the Port of Galveston, the American National Insurance Company, the University of Texas Medical Branch, and the beach front. Should The Strand's revitalization proceed, and the rail service be resumed to Houston, a pattern of movement among these areas would cross on The Strand. It is most desirable to establish a movement system operating between these economic generators. An elephant train or minibus line or other system of public movement should be developed. Failing this, a private system to service all or some of the points should be explored.

The establishment of the proposed movement system would aid The Strand greatly by making the entire area immediately accessible to much of Galveston. It could greatly reduce the dependency of this area on the automobile. This, in turn, would reduce the need for parking space and allow a greater building coverage ratio in the area. A high degree of building coverage is important if the area is to avoid the "snaggle Toothed" look of alternating buildings and vacant lots which some urban renewal projects have developed. The partial success of Larimer Square in Denver has led to the tearing down of additional surrounding buildings for parking. This, in turn, has created an isolation which threatens to destroy Larimer Square itself. It is vitally important for The Strand to maintain its continuity with other buildings in downtown Galveston and along the waterfront.

Strand revitalization will generate an increased need for parking. No single approach to this problem will suffice. Large cleared blocks of parking which destroy the urban feeling of the place should be resisted. Several opportunities which need to be pursued are:

- (a) Parking garages on Water and Mechanic Streets.

- (b) Diagonal street parking at 21st, 23rd, and 24th Streets between The Strand and Mechanic and Strand and Water Street.

(5) Pedestrian Movement

Just as important as transit and other vehicular movement is pedestrian movement within the general area of The Strand. The surrounding streets must be reasonably active with people during most parts of the day and night. Mixed land uses including housing and small commercial uses such as restaurants and clubs must be encouraged. Large open people-less places around The Strand or between it and other nearby important activity generators would be distinctly discouraging to free and confident pedestrian movement. The pedestrian should have a sense of security and should be provided with a pleasant and reasonably protected place to walk. The provisions of shade and adequate surfaces to walk on, as well as easy and graceful transition between level changes such as curbs, is most important.

(6) Traffic Movement

If The Strand is to achieve its full potential for becoming a prime link between the important economic generators of the city, it should remain a two-way street. If The Strand were one way it would tend to attract through automobile traffic that would not help support or service The Strand area. Traffic movement to and from the Medical School and the Santa Fe Building along Strand would help establish the street as the prime focal point it was historically. If this movement is largely by public transportation, moving groups of people, it would not disrupt the area but would tend to unify and strengthen it.

(7) Historic Preservation on Galveston Island

The program to restore and revitalize all of historic Galveston Island needs to be strengthened and continued. A major step in this effort is the completion of a comprehensive historical survey of the island. The survey is the joint work

of the Galveston Historical Foundation and the City of Galveston planning staff. Results of this survey should be published in a convenient form for public use. One of the benefits of the survey should be recommendations for the establishment of additional residential historical districts.

(8) Community Awareness

Community awareness of the importance of historic Galveston is critical to the success of The Strand project and the saving of additional historic areas. The historical survey should be used in conjunction with specially developed programs for the media that will inform citizens of the potential benefits of historic preservation. These should focus on the education aspects regarding Galveston Port and its regional importance, as well as on the benefits which have been derived from restoration programs in other cities such as New Orleans and Savannah.

(9) Bicentennial Program and The Strand

The bicentennial program now in progress has identified the historical importance of The Strand as well as the total island. The Strand planning effort is aimed at substantial completion of the restoration of The Strand by July 4, 1976. The bicentennial program is spearheaded by the Galveston County Cultural Arts Council, Inc. Goals of the bicentennial program which will strengthen the Strand project area are:

- a. Operation of Ashton Villa for public tours, civic events and a visitors center.
- b. Restoration of the Grand Opera House and Hotel built in 1894 for use as a performing arts facility, along with accommodations for visitors.
- c. Reclamation and renovation of historical and architecturally important homes, buildings and areas for needed housing, tourist attractions, and environmental control.

Such preservation will include that of St. Mary's Cathedral to be completed by 1976 and the preservation of Central High School, the first black high school in Texas, for use as a community center and historical museum.

These along with other goals of bicentennial program have clearly outlined the challenge for historic Galveston.

(10) Zoning

The Strand at present is zoned (L.I.) Light Industry. This zoning designation is open to many normal uses but also permitted within it are: trailer camp and mobile home park, electric power generating plant, contractors storage or equipment yard, drive-in theatre and convent. Consideration should be given to changing the zoning to be more in keeping with land use recommendations set forth in a recent report of Tom Price, "Historical Development for Galveston, Texas, 1973". One way to accomplish these land use goals would be by extension of the central business district zoning. Existing uses would not be altered by this extension but additional non-conforming uses would not be permitted.

Conclusion

The Galveston Historical Foundation is responsible for the planning effort to restore and revitalize The Strand. In undertaking this responsibility, the Foundation works in close cooperation with the Galveston County Cultural Arts Council. It is not economically possible, however, for the revolving fund of the Historical Foundation to purchase all of these buildings, restore them and maintain them, therefore, it is crucial to involve other individuals and groups both public and private in the revitalization of the Strand Area. In this sense cooperation between the Galveston Historical Foundation and the city is of primary importance in accomplishing the proposals for street lighting, landscaping, sidewalks/paving, and signage which are outlined in this study.

The Strand, once the center of commerce for Galveston and all of Texas, is no longer a vital factor in the daily lives of most Galvestonians. It has the potential to be the link between downtown and the wharf area, to be a historical landmark, to be a place for celebrations and festivals, and to be a new shopping, office and residential center.

X A new transportation system could move people from the beach, through the Strand/downtown mall area and University of Texas Medical Branch. The wharf area waits to become another Ghiaardelli Square of San Francisco, the vacant historic buildings on Strand, Mechanic and Water Streets wait to be bought or rented for a fraction of the cost of a similar building in Houston. Houston's one and one half million residents are 45 miles away. Galveston has sunny skies and the historic character of a great cosmopolitan city.

The ideal vehicle for this revitalization of the city lies in continuation of the restoration/preservation that has already begun in the residential district and is now occurring in both the historic buildings and streetscape of The Strand. The restoration will return the street and its buildings to the historic spirit of the area. The preservation will ensure that the deterioration of existing buildings streetscape will

be stabilized at this time.

The Strand from 20th and 25th has lost major buildings in each block and particularly at intersections. This destruction has naturally caused the area to lose a significant portion of its urban character. The parking lots that now exist where fine buildings once stood are sad indications of the economic decline of the area. New buildings that should ultimately occur in these vacant spaces will require certain architectural controls. New controls should limit height, color, and exterior openings of a building as well as other aspects of construction. Within the architectural vernacular of the existing historic buildings there is a potential and challenge for "modern" architecture.

Before the new buildings occur, however, the existing buildings on The Strand should be revitalized with new activity and uses. The Galveston Historical Foundation, which now owns six buildings on Strand, must continue the quality of exterior restoration/revitalization that has occurred with the First National Bank and Truehart-Adriance Building both located at 22nd and Strand. Contracts of sale for these Galveston Historical Foundation buildings will require restoration of facades and sidewalks to the curb line with work to begin within a time period agreed upon at the time of purchase.

All proposals and designs will be reviewed and approved by the Galveston Historical Foundation Steering Committee with advice from an Architectural Review Board. The review board should be made up of noted architects and architectural historians who should meet two or more times a year to review progress in the project.

The restoration/revitalization of The Strand should accomplish the Galveston Historical Foundation's goal of preserving the historical buildings, but equally important are the community benefits. The community would see:

1. An increase in property values
2. Greater tax returns for the city

3. More private investment
4. An increase in tourism, and
5. Local awareness and pride in the Strand Area.

Once this process of renewal, preservation and adaptive use is demonstrated along The Strand, its influence will spread to all of Galveston's historic areas.

IV. Visual Concept

Any attempt to settle on a particular period for the "style" of The Strand restoration will be subject to controversy. Some of the earlier remaining buildings which date from pre-civil war days are simple and classic in their purity of design. On the other hand, the last of the Strand buildings of interest today are late nineteenth and early twentieth century buildings which are elaborately and floridly Victorian. By the time these buildings were built, the earlier ones were surely out of favor and perhaps even somewhat rundown.

Rather than pick a particular time period for restoration, it is more appropriate to settle on a concept for development that embodies principles which recognize a plurality of style and are ongoing and thereby self-renewing. Each element of The Strand should be judged in relation to the others, but also on its own visual merits. There would thus be a place in The Strand of the future for new construction, respectful of its neighbors, but also of distinguished creative design. This concept allows a kind/^{of} renewal which would provide long range stability through normal economic processes.

One possible goal for a specific building would be to restore each to its original appearance. Although this might be ideal, it is not practical for all buildings within the present concept of Strand development. The Strand proposal envisions changes of use which may require some changes in exterior appearance. Also, buildings that are adapted to protect against high water may require some modification^{of} their facades. Many canopies appear to have been added to existing buildings in the late nineteenth century. It is interesting to note that some of these buildings which did not originally have canopies, but to which canopies were added, did look better with canopies. The reverse of this may be true: that some buildings once having canopies might look much better without them.

The Moody, Mensing and Sealy Building all had canopies that shadowed their handsome first floor facades. Because of these factors, it is recommended that each building be restored to the way it looks best, as long as it is in the spirit of its historic character.

Within this spirit the following principles are fundamental:

1. A serious attempt must be made to properly restore each building on The Strand.
2. Avoid setting a particular date or "period" for restoration on The Strand.
3. Restore individual buildings in accord with the way each will best appear, so long as it is in harmony with its neighbors and its historic character.
4. Foster a spirit of continuing investment and preservation of all significant architecture on Strand.

A basic concern in the visual concept for The Strand is to establish the continuity of the street from 20th to 25th. This would allow the street to work as a unified whole for the person who might experience The Strand by foot or vehicle. The key problem with The Strand in its present form is that many of the fine original buildings have either been significantly altered or torn down. Each block suffers from this fact and while many fine buildings remain, they will become isolated jewels if the street can not again be given continuity. The street elements of lighting, paving, signage and landscaping can never accomplish the effect of continuous handsome facades. They can, however, supplement both the remaining good buildings and other non-historic buildings. They can also work as a transitional element between buildings and blocks.

Lighting is an immediate need for the area and human scale lights when spaced evenly along Strand provide this first needed continuous element. If original fixtures from the city were to be found or recast, this

would add an elegant and traditional element of historic Galveston. It would also identify the street as the starting point for preservation/ restoration efforts in the downtown area. If original style fixtures are not available, then appropriately simple contemporary fixtures would serve the same purpose of continuity and would add the dimension of new and old enhancing each other. The important design element of the fixtures is that they are both human in scale and evenly spaced along the blocks. Intersections would become bright spots with more fixtures occurring at these points.

Street paving, sidewalks and curbs offer the next element of continuity on The Strand. The street, if it were returned to its original brick surface, would bring a strong and beautiful pattern and texture to Strand. The existing asphalt paving, due to its great width, is a dominant negative element now but this could be reversed with the return to brick. Though expensive, this return should be kept in mind as a long term future project.

Curbs and sidewalks are a most important aspect of visual continuity of the street. The original curved brick curbs, evident in some places, also exist under a thin concrete topping. Many of the original sidewalks remain even though the building to which they belonged may be gone. While curbing is similar throughout The Strand, the sidewalk type is constantly changing with individual buildings. The sidewalk historically was a reflection of the various buildings and the richness and variety of these sidewalks give a pedestrian a true sense of the historic Strand. The retention of this delightful mixture of styles and materials should be retained.

Original signs along the Strand were of fine graphic quality and were usually mounted flat against building facades or canopies. New signage can offer continuity much like the various sidewalk materials. Signs can be totally different in character, lettering and color and yet, if of limited size and lighting technique, can work with other street elements so that each is a part of a refined yet diverse system. Within these signs one can visualize banners and flags which

give movement, color and announce various events that are to occur.

Landscaping is another element which can help provide continuity to The Strand. Street trees evenly spaced along the Strand would again connect the area from 20th to 25th. Even trees placed only at "gaps" caused by vacant lots and a few significant buildings would greatly enhance the street scene. Trees would provide the shade given by the historic canopy lined street and also provide a new element for The Strand more in keeping with proposed building uses. The street tree would act as the final element in unison with lights, curbs and paving to form a continuous Strand. Other landscaping can also give life and variety to The Strand. Parking lots and vacant lots should be buffered by trees, shrubs and flowers. Small "vest pocket" parks with a variety of vegetation might also find a place in vacant spots between buildings.

Lighting, paving, signage and landscaping are the common design elements key to The Strand. They add that most important touch of meaning and human scale characteristic of all great streets and districts. In combination with successful preservation/restoration of the many fine buildings Strand will again be an entity. As to proper use of The Strand, it simply must again be a place for people; for the many at a "Festival on The Strand" and for a strolling solitary person.

The implementation of a successful public/private program to insure the provisions of these primary street elements is critical. This program has a great value in publicly manifesting progress along The Strand. The proper timing and exploitation of these activities will boost the sales of individual buildings to investors and the project to the Galveston Community.

V. Restoration Statement

The Galveston Historical Foundation will require restoration/revitalization for buildings that come under its control. The following definitions are provided to clarify terms that might occur in documents and discussions:

1. Restoration - to return a structure, authentically, to its previous form at a particular date or period.
2. Reconstruction - the recreation of a building, parts of a building, or a site from historical, architectural or archaeological documents and other sources of researched information.
3. Preservation - the stabilization of a building to prevent further negative change or deterioration of its present condition.
4. Rehabilitation/Renewal - the redefinition of a building's use to preserve features which are independent of any particular structure or period of time. That is to say a rejuvenation of an older structure through a change of utilization.
5. Revitalization - the upgrading of a building or an area by using any or all of the above forms of Historic Preservation so that the building or area becomes a (1) a social (2) aesthetic (3) cultural and (4) economic value to its community.

Each building will present its own particular problem and challenges to the appropriate type of work to be done and thus each should be judged individually.

Standard Work to be Accomplished:

1. Building facades as a general rule will be restored to their original appearance, typically in the mid or late 1800's.
2. Historic buildings on the Strand retain much of their original detail; cast iron columns, masonry details, window details, sidewalks and curbs. All have the potential for restoration. Both cornice and first floor, however, have suffered from hurricane and flood damage and this may limit some restoration efforts.
3. Historic buildings will generally require the cleaning, painting, and straightening of cast iron columns cast iron window details and the repair of damaged stucco or masonry.
4. The facade of each building should be considered to extend to the curb line and thus all work should include the facade, and sidewalk.
5. With a history of hurricanes that have caused flooding to approximately a 3½ foot level throughout Strand, the first floor of buildings will need special consideration. This condition might lead to buildings with modifications from their original appearance.

General Outline for Restoration/Revitalization Work For Each Building

I. Research and Drawings

- A. Research to determine the original appearance of the building and/or how it has been altered since original construction.
- B. Select period of facade restoration.
- C. Complete drawings to illustrate restoration or new design for facade.

II. Elements of Construction and Restoration

- A. Curbs: Repair/replace original brick (usually brick but sometimes stone).
- B. Sidewalks
 - 1. Repair/replace original walks.
 - 2. Select appropriate materials for new walk.
- C. Facades
 - 1. Cornice, cast iron columns and cast iron window details.
 - a. Clean and repair existing members.
 - b. Replace destroyed members with cast aluminum or polyester plastic.
Use molds from existing members if available.
 - c. Paint all exterior cast iron.
 - 2. Wood Doors and Windows
 - a. Clean & repair existing members.
 - b. Replace existing members when necessary.
 - c. Paint all exterior wood members.
 - 3. Masonry
 - a. Repoint existing brick.
 - b. Clean and seal existing brick.
 - 4. Stucco
 - a. Repair and patch exterior stucco.
 - b. Paint exterior stucco.

D. Miscellaneous Items to be Considered:

1. Foundation Conditions
2. Flood Level
3. Interior Party Wall Stability
4. Roof Leaks
5. Existing Mechanical System When Applicable
6. Alley Facade Condition
7. Interior Columns
8. Floor Construction

Galveston Strand

VI. STREET LIGHTING

General Statements

Historically The Strand was lighted by gas with at least two different types of light fixtures being utilized (see photographs). When in the 1880's electric arc lighting was introduced to supplement the gas lights, the electric lighting had the disadvantage of overhead power lines running along The Strand.

Today one overhead electric light hangs at nearly every intersection of The Strand between 20th and 25th Streets, and the overhead power lines have been moved to the alleys (hopefully to be placed underground as soon as possible). In large part, however, The Strand is unlighted, and thus at night the facades of the historic buildings cannot be seen and pedestrians are unable to move easily along the sidewalks.

In attempting to recapture the ambiance of The Strand and to create an attractive area for pedestrian use, the Foundation has decided to recommend period gas lighting as its first choice. Gas lighting has several advantages for The Strand: first, it casts a soft light which is attractive for pedestrian use; second, it is historically authentic for The Strand with the type of fixtures recommended; and, third, Southern Union Gas Co. has worked aggressively to make the cost attractive by agreeing to donate \$8,000.00 in work and to obtain a 30 to 40% discount on poles and fixtures for The Strand.

Gas lighting does have the disadvantage of providing less quantity of light, as opposed to quality of light, than electric lighting with similar poles and fixtures. The Foundation is satisfied, however, with the assurance of its architects and Southern Union Gas that the gas lighting will be adequate for comfortable pedestrian use if the spacing of fixtures and

number of mantels are effected as proposed herein.

In evaluating different types of gas light fixtures the Foundation was torn between two types, both of which happen to be in use in Galveston today. The Wellsbach Boulevard is the light at the Flagship Hotel¹ and two of the Charniglow 9000 Model stand in front of the Stewart Title Building near The Strand. After careful consideration the Foundation has decided upon the 9000 Model. It was felt that the 9000 Model complemented the historic buildings without distracting from them as the more striking white and black contrasts of the Wellsbach might do. The 9000 Model is also slightly less expensive per unit than the Wellsbach. Finally, both the Wellsbach and the 9000 Model are very similar to the gas fixtures actually used on The Strand in the 1800's, as evidenced by the photographs herein.

Photographs of the 9000 Model appear below. Both the poles and light fixtures will be identical to those standing in front of the Stewart Title Building. Six poles and fixtures are recommended for each side of The Strand per block, or twelve poles and fixtures total per block. Four mantels will be used in the mid-block fixtures, while eight mantels will be used for the four fixtures at each intersection in order to accent the street corners. Twelve additional units are included for lighting on cross streets where restoration has occurred or is in process. The estimated capital cost and proposed method of funding are attached.

1. These lights are now converted to electricity because of the buffetting the direct sea wind gave the gas mantels.

STRAND STREET LIGHTING

I. Capital Cost of Lighting Project

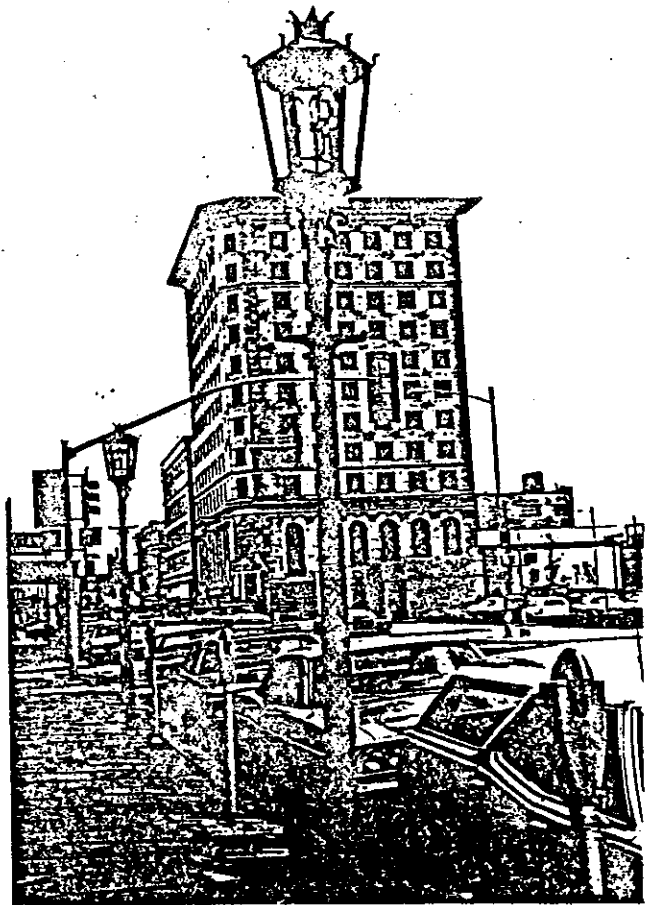
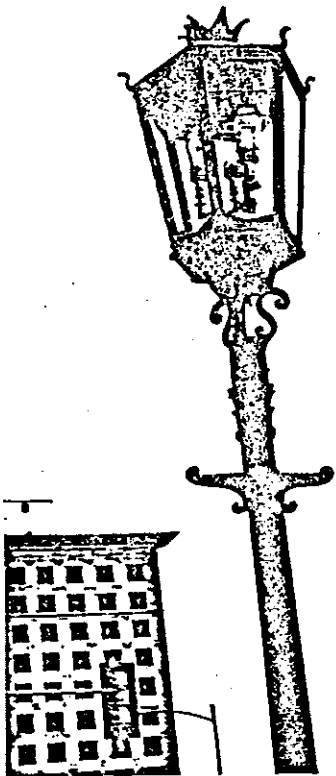
1. Purchase of 72 Charmglow 9000 fixtures and poles	\$25,900.00
2. Extension of gas lines to each pole	8,000.00
3. Installation of poles and light fixtures	4,000.00
4. Alley lighting and contingency fund	<u>5,000.00</u>
TOTAL	\$42,900.00

II. Funding of Capital Cost

1. Contribution by Southern Union Gas Company of extension of gas lines to each pole	8,000.00
2. Contribution by City of Galveston	4,000.00
a. Cash contribution	6,000.00
b. Installation of poles and light fixtures	4,000.00
3. Payment out of Revolving Fund	5,000.00
4. Voluntary subscriptions by Strand property owners @ \$2.00 per foot of Strand frontage (GHF subscription is included in #3 above)	4,000.00
5. Grant by Moody Foundation (Requested)	14,400.00
6. Grant by Kempner Fund	<u>1,500.00</u>
TOTAL CAPITAL COST	\$42,900.00

III. Maintenance and Operation

1. Provision of mantels - donated by Southern Union Gas Co.
2. Payment for gas used at flat rate - City of Galveston:
72 fixtures maximum with 62 using 4 mantels and 10
using 8 mantels =
328 total mantels X \$.61 per mantel per month =
\$200.08 total per month X 12 = \$2,400.96 per year.
3. Maintenance - City of Galveston



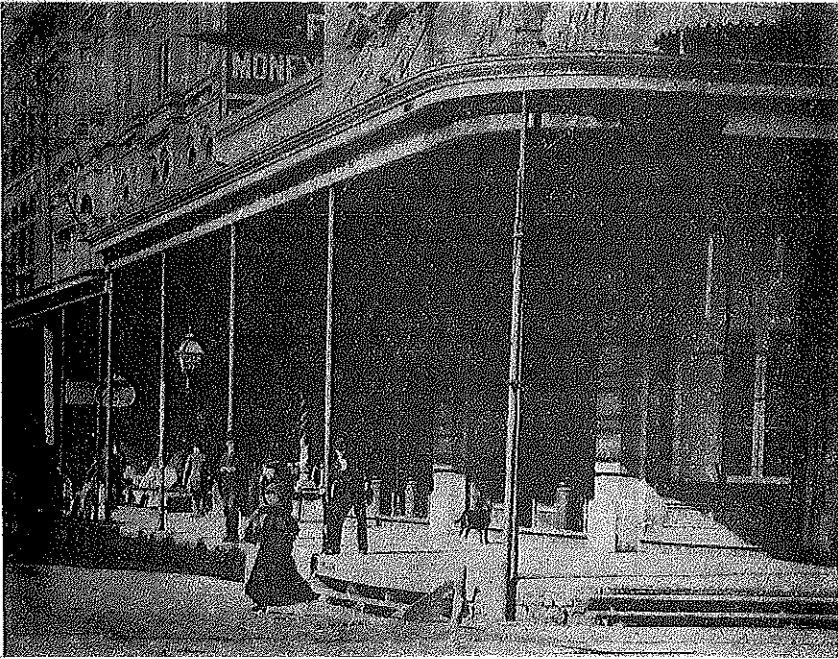


EXHIBIT 3 :

GAS LIGHT ON CANOPY COLUMN

AT MARKET AND 22ND

CIRCA 1893

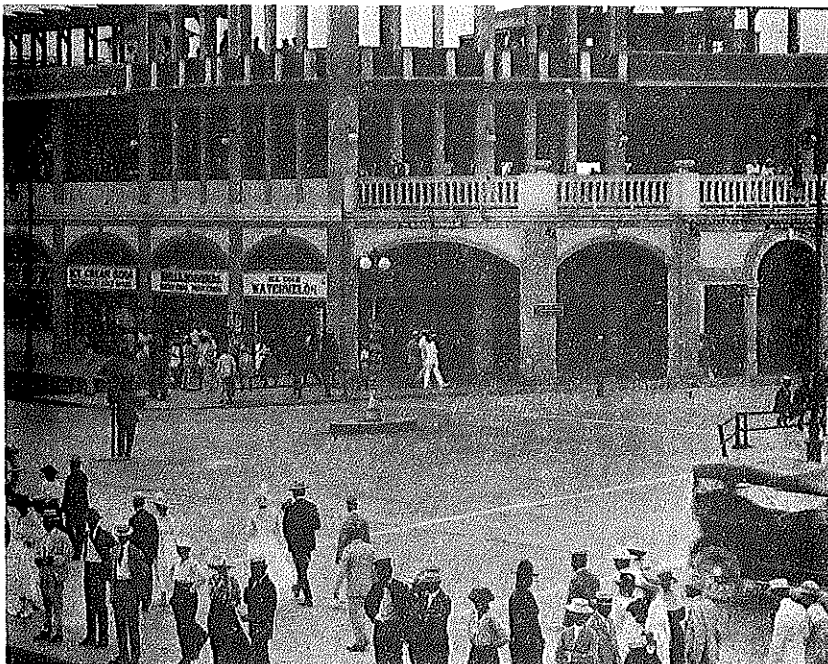


EXHIBIT 4:

ELECTRIC STREET LIGHTS AND

BUILDING LIGHTING AT 23RD

AND SEAWALL BLVD.

CIRCA 1925

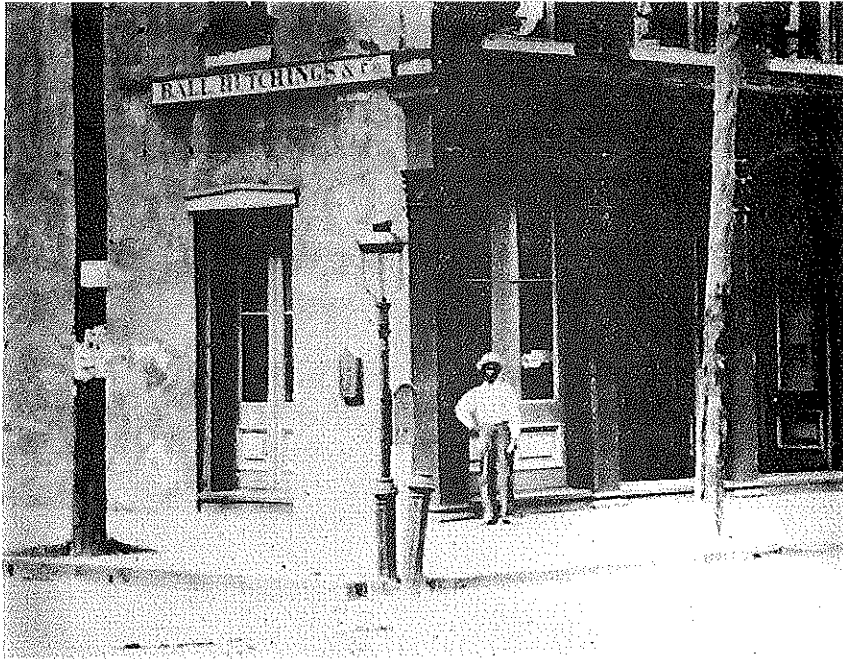


EXHIBIT 1 :

GAS LIGHT AT THE BALL HUTCHINGS
AND CO.

STRAND AND 24TH

CIRCA 1885

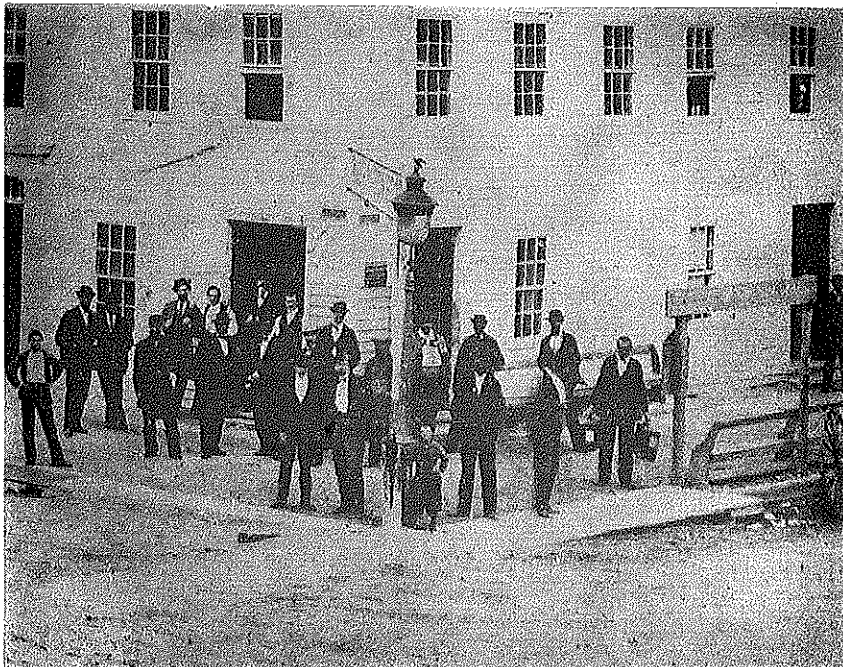


EXHIBIT 2 :

GAS LIGHT AT MECHANIC
AND 21ST

CIRCA 1875

VII. Galveston Strand

SIGNAGE

General Statement:

Existing building use and disuse has left The Strand with a limited number of signs. While none of the existing signage detracts significantly from the overall character, it does not add to the historic character of the area. New signage must be designed to compliment the renovation that will occur.

In this respect the use of the existing sign control ordinance might in some ways limit the potential for giving life and restoring the graphic portion of the old character to the area. A liberal signage attitude must be balanced again over the indulgence of the McDonald's golden arch concept that seems established in the American commercial enterprise.

The exhibits 5 thru 8 illustrate the various types of signage in use during the "historic" period of the Strand area. From these some excellent examples can be drawn as models for judging new signage proposal. Graphics (lettering styles) historically and wide variety but a general style of serif letters appeared in small scale buildings signs. Large signs (the west side of the Moody buildings) used a variety of serif and sans serif letters. The "Washington Hotel" type sign is also large type sign that was incorporated with the facade of the building.

The existing street signs only redeeming design feature is their legibility. As with most traffic signage they show no thought for design or detail that might be in character with the historic area. The street signs should be replaced. A representative of the City of Galveston stated that this is possible but the city will not help in cost of replacement.

The existing traffic control signage presents a problem of sign clutter because of one way street systems that connect to Strand. This type signage is, however, being replaced with international symbols which will improve its design character. Traffic control signage along Strand is primarily oriented to parking control with numerous signs and painted curbs. This system should be replaced with a simple uniform pole mounted designation.

Recommendations:

Building Signs: Signage of buildings acquired and sold by the Historical Foundation should come under deed restrictions. The Steering Committee of the Galveston Historical Foundation will give approval for all signage. Each building when possible should be researched and the "period" exterior graphic elements identified. This could be done with historic foundation research group which developed information for this project. Each building, however, will use signage according to each particular use or uses and thus research might be applicable only in the historic sense.

For the general criteria for the historic area signs guidelines, please see the "Historic Area Guidelines" in Exhibit #9. It should be noted that this regulation is directed at a residential area and the Steering Committee should be open to signage that would vary from the regulations in specifics but not intent.

Also, it should be clear that these regulations apply to only buildings bought and sold by the Historical Foundation.

Wall signs from the historic period (Moody Building, west wall) should not be altered or improved. Wall signs of recent vintage (the First Hutchings Sealy National Bank Parking Lot Sign) should be removed by painting or sandblasting.

Street Signs: Existing street signs (typical green and white) should be replaced with new signs more appropriate to the character of the Strand (see Drawings #3 & 4 p. 53 . These new street signs could be (1) pole mounted on new lighting post (2) wall mounted on buildings as occurs in London or (3) pole mounted on existing street sign poles. Any changes would have to be approved by the city.

Street signs for wall mounting (size approximately 12" x 24") should be porcelain-enamel signs with silk screen letters and symbols, (see Exhibit 10) cost + \$75.00/sign.

Street signs for pole mounting should be similar in construction but smaller in size (approximately 6" x 20").

Traffic Control Signs: This type sign should be replaced with the international symbols when possible. The city is responsible for this signage.

EXHIBITS 5 thru 8

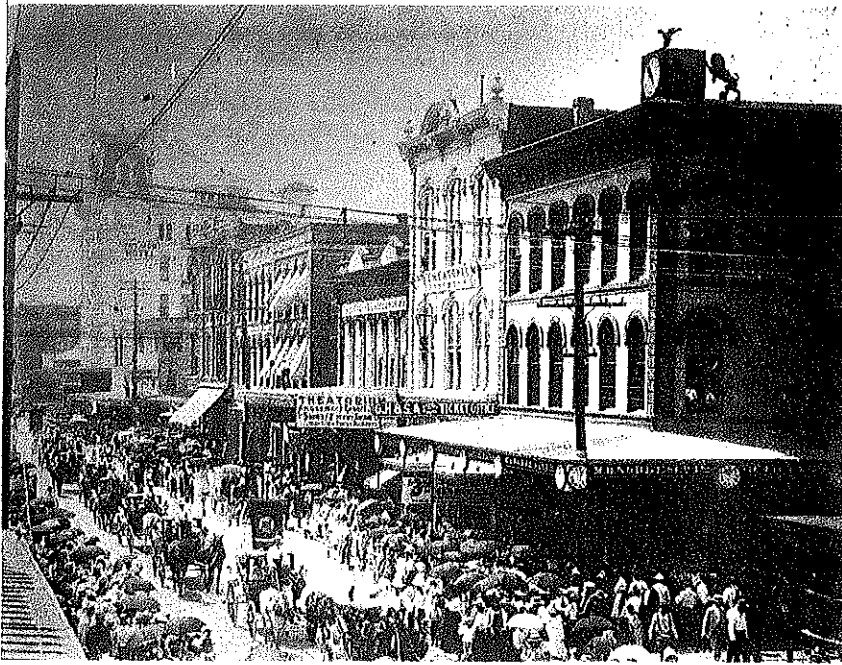


EXHIBIT 5 :
SIGNAGE ALONG TREMONT ST.
CIRCA 1910

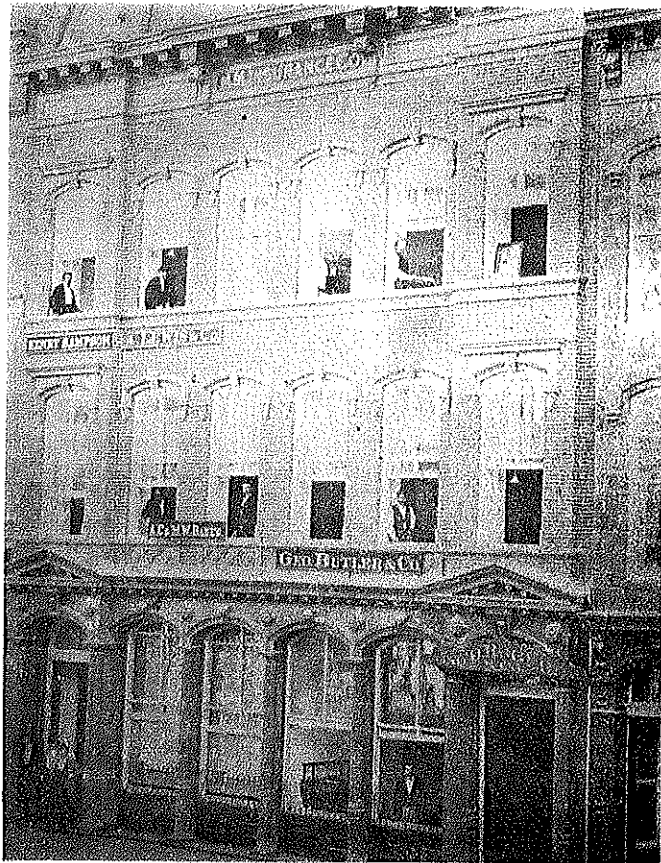


EXHIBIT 6 :
SIGNAGE AT THE MERCHANTS
MUTUAL BLDG.
2317-19 STRAND
CIRCA 1875

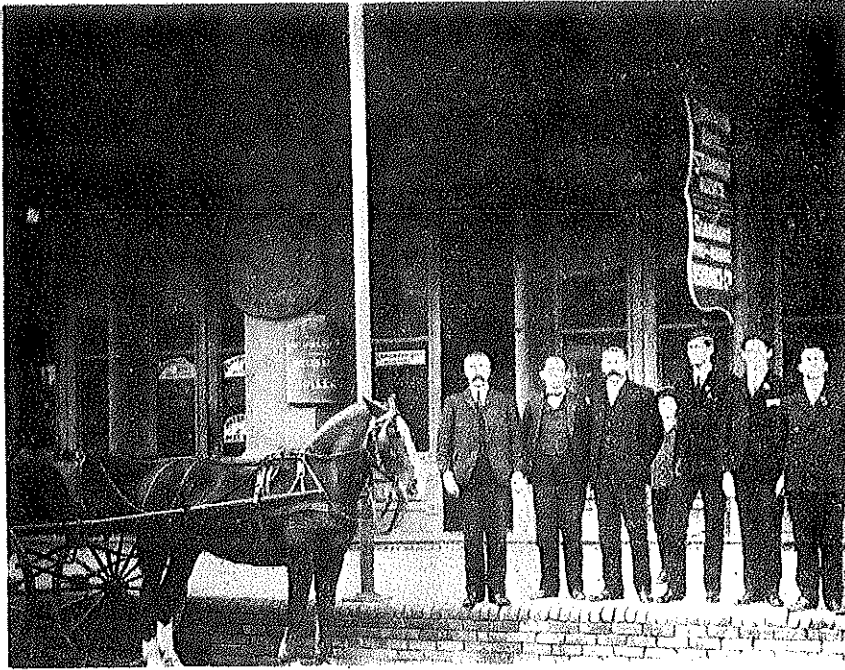


EXHIBIT 7 :

SIGNAGE AT THE ADOUE LOBIT

BANK BLDG.

STRAND AND 21ST

CIRCA 1906

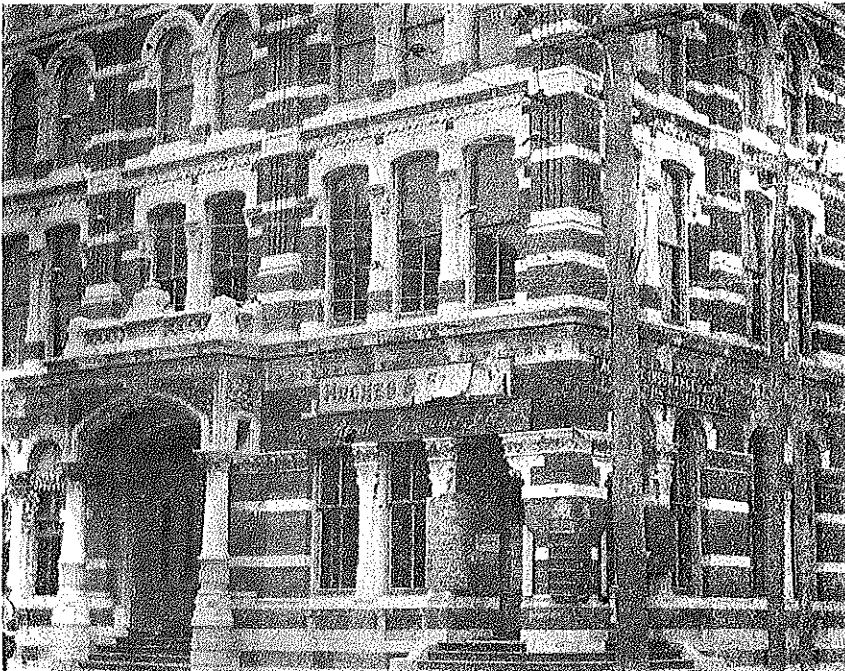


EXHIBIT 8 :

SIGNAGE AT THE COTTON

EXCHANGE BLDG.

MECHANIC AND 21ST

CIRCA 1890

Exhibit #9

HISTORICAL AREA SIGN GUIDELINES FOR THE STRAND
Requirements for GHF purchased buildings.

1. DEFINITIONS

The following terms, as used in this guide, are hereby defined as follows:

(a) SIGN shall include any symbol, device, image, poster, banner, billboard, design or directional sign used for advertising purposes, whether painted upon, attached to, erected on, or otherwise maintained on any premises, containing any words, letters, figures, numerals, phrases, sentences, emblems, devices, trade names, or trade marks, by which anything is made known, such as are used to designate an individual, a firm, an association, a corporation, a profession, a business, a commodity or product, which is visible from any public thoroughfare and is used to attract attention.

(b) DISPLAY includes erect, paint, repaint, replace, hang, rehang, repair, maintain, paint directly on a building or other structure, inlay imbed in or otherwise exhibit in public view.

2. GENERAL PROHIBITION OF MISCELLANEOUS SIGNS

The display of signs of a miscellaneous character visible from public thoroughfares within the Historical District, except as otherwise provided for in Section 25-17 of the Zoning Ordinance, is prohibited.

3. SIGNS MUST CONFORM TO CHARACTER OF HISTORICAL AREA

In addition to the regulations contained in the Zoning Ordinance, approval of the display of a sign in the Historical Area shall be granted by the Galveston Historical Foundation only when such signs and the plans therefore, relating to the appearance, color, size, position, method of attachment, texture of materials and design, conform to the

distinctive character of the District or do not injuriously affect or impair the value of those buildings having architectural or historical worth.

4. WHAT SIGNS MAY ADVERTISE

No sign of any character shall be displayed in the Historical District unless such sign advertises a bonafide business conducted in or on the premises and not more than fifty percent of the area of such signs may be used to advertise products or commodities actually sold on the premises.

5. NUMBER OF SIGNS ALLOWED

One sign only shall be allowed to each store, shop or bonafide place of business, and this sign shall be no larger than the maximum stipulated in this guide, regardless of front footage of the property.

6. PROHIBITED SIGN PLACEMENT

No sign shall be placed upon a balcony, gallery, canopy, shed, roof, door or window or placed in any manner whatsoever so as to disfigure or conceal any architectural feature or detail of any building. No sign shall be displayed from any fence, wall or open lot unless it conforms in proportion to the allowable area and does not exceed the maximum.

7. PERMITTED PROJECTION OF SIGNS

No sign shall project more than forty-eight inches beyond the building line, except that, for the purpose of illumination, a hood may be used but not to exceed six inches additional projection.

8. SURFACE AREA OF SIGNS

The surface area of any sign shall be in direct proportion the amount of front footage of each ownership and shall be as follows:

- (a) For single-faced signs, attached flat against the wall and including painted wall signs there shall be allowed thirty square inches of sign surface area for each foot of lot frontage.
- (b) For double-faced signs, projecting or detached, there shall be allowed sixty square inches of sign surface area for each foot of lot frontage. The area of such a double-faced sign shall be taken to mean the sum of the areas of each face.
- (c) In no case shall the area of a single-faced or painted wall sign exceed eight square feet, the maximum allowed size for any such sign.
- (d) In no case shall the area of a single-faced or painted wall sign be less than two square feet, unless by special permission of the Historical District Board.
- (e) In no case shall the area of a double-faced sign exceed a total for both faces of sixteen square feet, the maximum allowed size for any such sign.
- (f) In no case shall the area of any one double-faced sign be less than four square feet, unless by special permission of the Historical District Board.
- (g) In the case where two or more businesses are conducted on the premises of single ownership having a front footage of twenty-five feet or less, the allowable sign area shall be increased by one and one-half times.

9. ILLUMINATED SIGNS GENERALLY

In the case of illuminated signs, where space must be provided between two parallel faces for the installation of lighting fixtures, these faces shall not be farther apart than eighteen inches and such lighting fixtures and all light sources shall be a steady, non-flashing, light concealed; (a) behind standard opal glass or other substance of equal or smaller light transmission factor; (b.) by hoods; or (c.) by any acceptable method of indirect lighting approved by the Historical District Board.

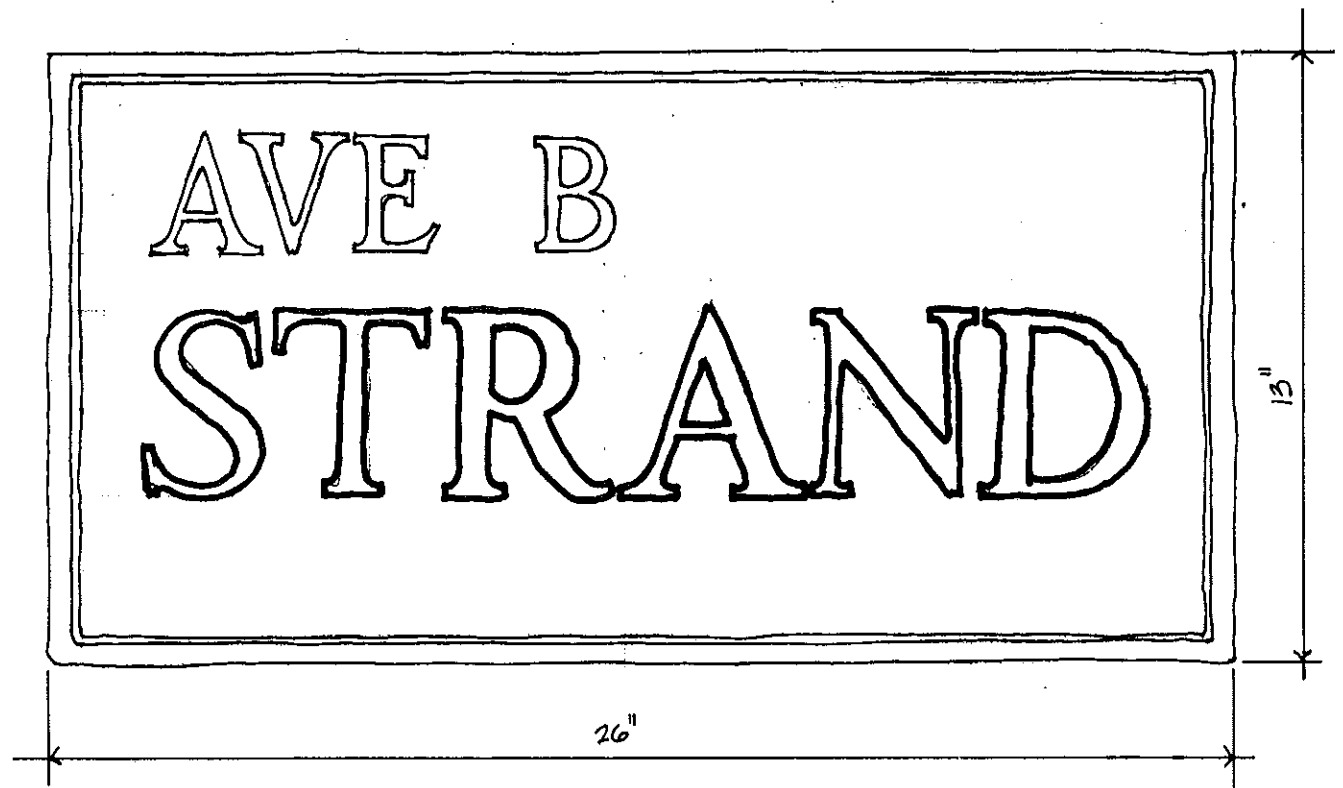
Note: Neon tube signs shall not be allowed by the Historical District Board.

10. BUILDING CODE APPLICABLE TO SIGNS

All signs in the Historical Area shall be further governed by the existing regulations of the Building Code of the City which are not in conflict with this guide.

11. APPLICATION FOR SIGNS TO BE SUBMITTED TO BOARD
All applications for permits to display signs within the Historical Area shall be submitted to the Historical Foundation before a permit therefor may be issued.
12. APPLICATION FORM FOR SIGNS;ACCOMPANYING DRAWINGS
Application for a permit to display signs in the Historical Area shall be made to the Director, Galveston Historical Foundation, upon forms furnished by the Planning Department of the city. Such application shall also be accompanied by sketches and drawings in triplicate showing details of construction and foundation when required by the Building Code of the city and shall delineate size, shape, design, coloring, lighting and position in relation to the building from or upon which it shall be displayed.
13. VIOLATING SIGNS, ETC., TO BE REMOVED
Any sign or exterior illumination of walls, exteriors, roofs, or appurtenances of buildings displayed contrary to the provisions of this guide shall be removed.

Drawings #3 and #4

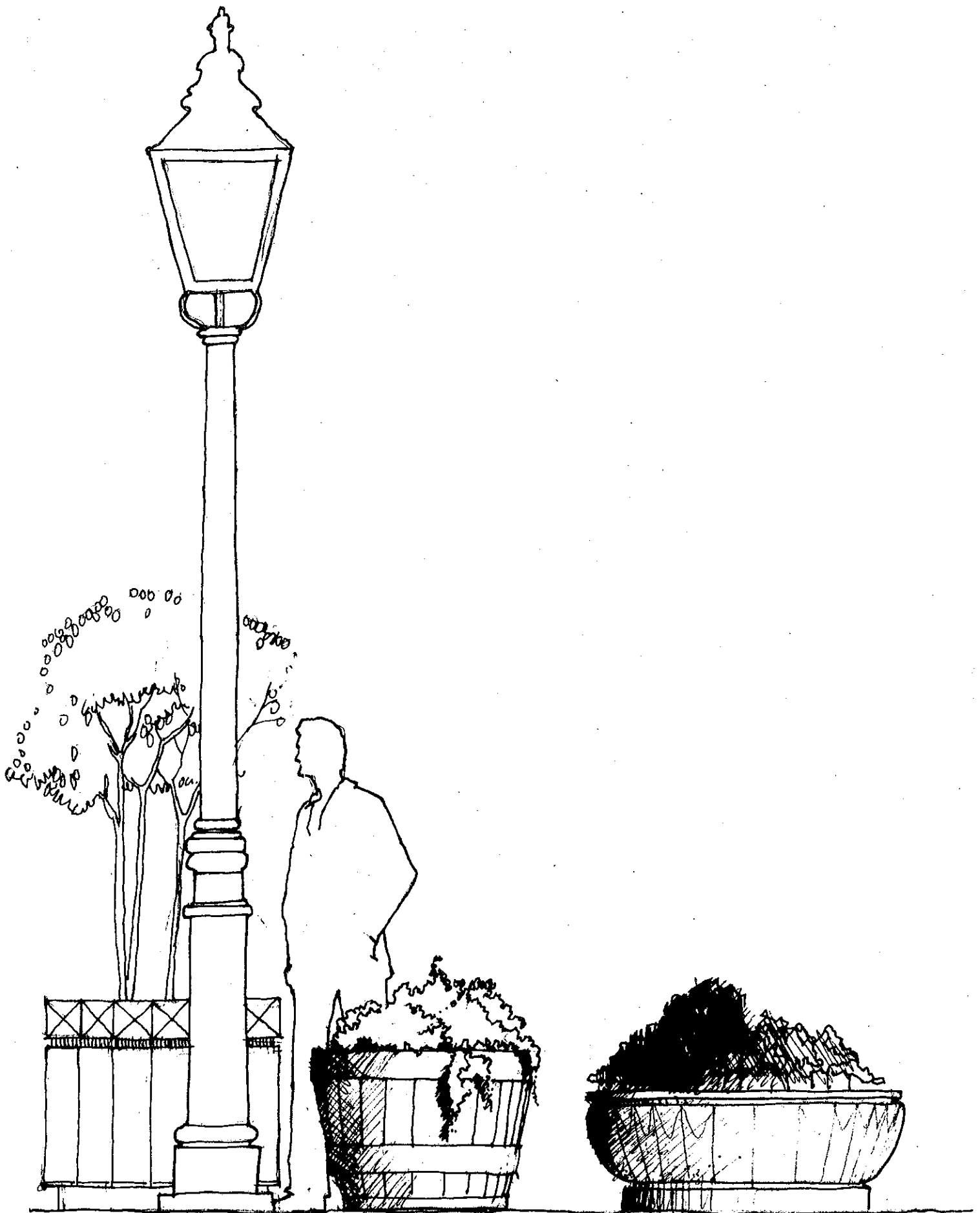


FORD
POWELL &
CARSON
ARCHITECTS
& PLANNERS

528 KING WILLIAM STREET, SAN ANTONIO, TEXAS 78204 (512) 226-1246

TYPICAL STREET SIGNS
GALVESTON STRAND

VII-12



LEAD COVERED WOOD

WOOD BARREL

CARVED OR CAST STONE

STRAND: PLANTING CONTAINERS

DRAWING NO. 15



Live Oak
(Q. virginiana)

Evergreen

40 feet high--twice as wide

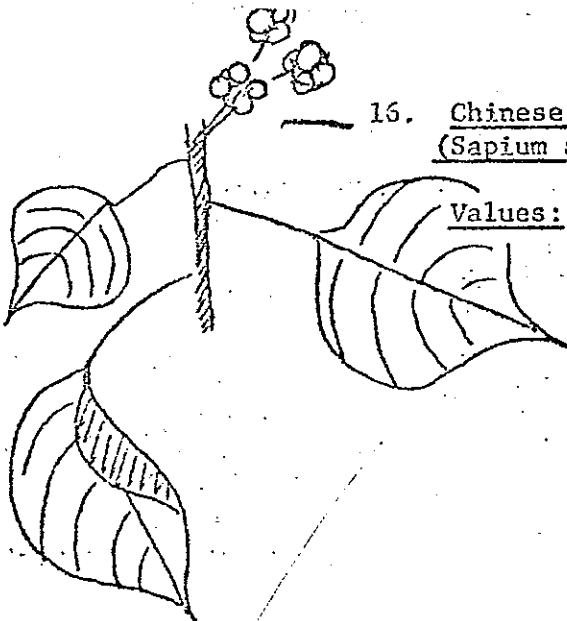
1. Easy culture
2. Vigorous root system may be damaging
3. Medium to slow rate of growth
4. Long lived



e. Shumard Red Oak Deciduous
(Q. shumardi)

60-80 feet high

1. Good red fall color
2. Long lived
3. Very clean in appearance
4. Easy culture



16. Chinese Tallow
(Sapium sebiferum)

Deciduous

30 feet high

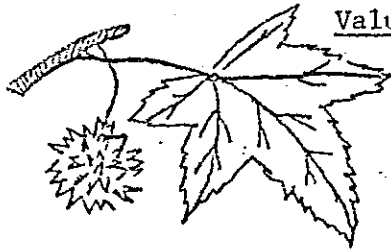
Values:

1. Excellent fall color
2. Rapid rate of growth
3. Easy culture
4. Attractive seedpod in fall

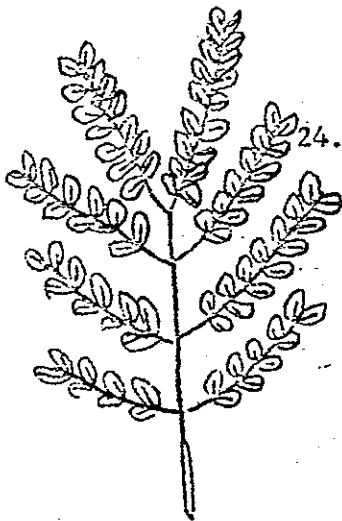
Drawbacks:

1. Seedpod may be messy
2. Severe cold may injure

*Sweetgum Deciduous 60-80 feet high
(Liquidambar styraciflua)



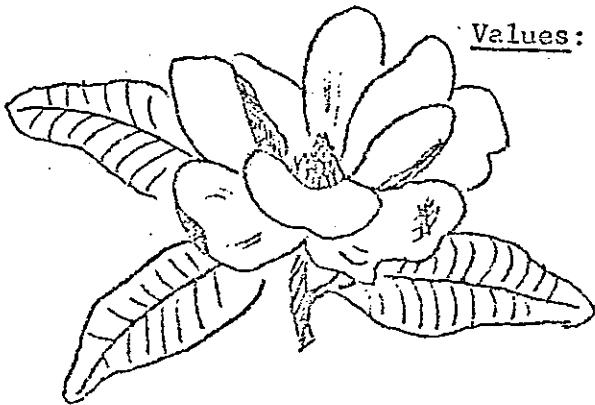
<u>Values:</u>	1. Fall color	<u>Drawbacks:</u>	1. Difficult to transplant
	2. Easy culture		2. Seed balls
	3. Upright, neat form		



24. Moraine Honey Locust Deciduous 15-20 feet high
(Gleditsia triacanthos moraine)

<u>Values:</u>	1. Fast growth	<u>Drawbacks:</u>	1. Weak wooded
	2. Attractive yellow fall color		2. Sometimes susceptible to borers
	3. Attractive-- fine textured-- yellow-green foliage		3. Demands sun

9. *Southern Magnolia Evergreen 60-80 feet high
(Magnolia grandiflora)



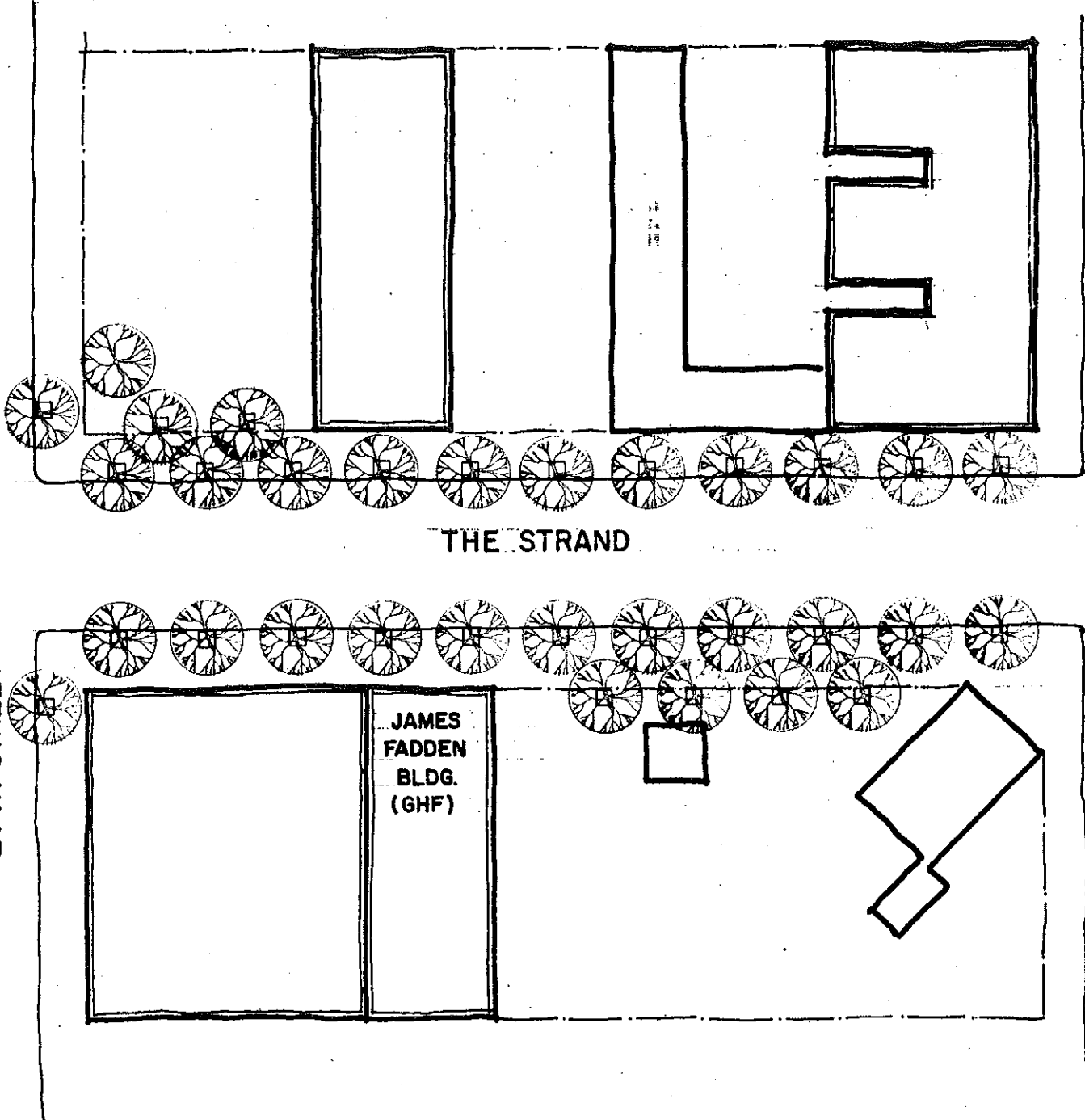
<u>Values:</u>	1. Long lived	<u>Drawbacks:</u>	1. Relatively slow growth
	2. Glossy, evergreen, coarse-textured foliage		2. Susceptible to scale insects and leaf diseases
	3. Large white, fragrant blooms beginning in early summer		3. Prefers acid, loose, organic soil
	4. Attractive seedpod		

Drawing # 17

Drawings 18 thru 23

24 TH STREET

25TH STREET



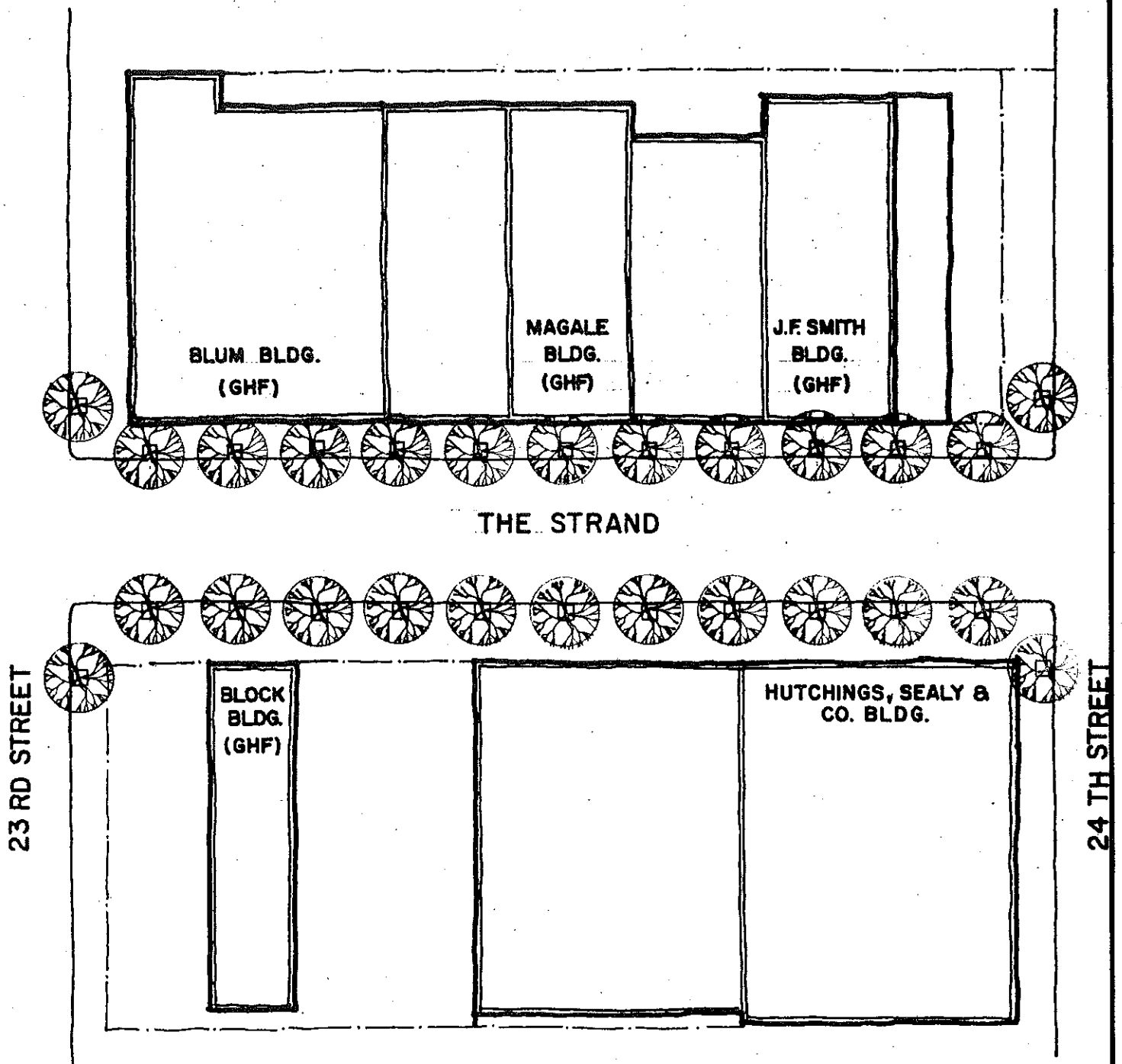
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STRAND: LANDSCAPING
24 TH TO 25TH

ALTERNATIVE I

IX-12



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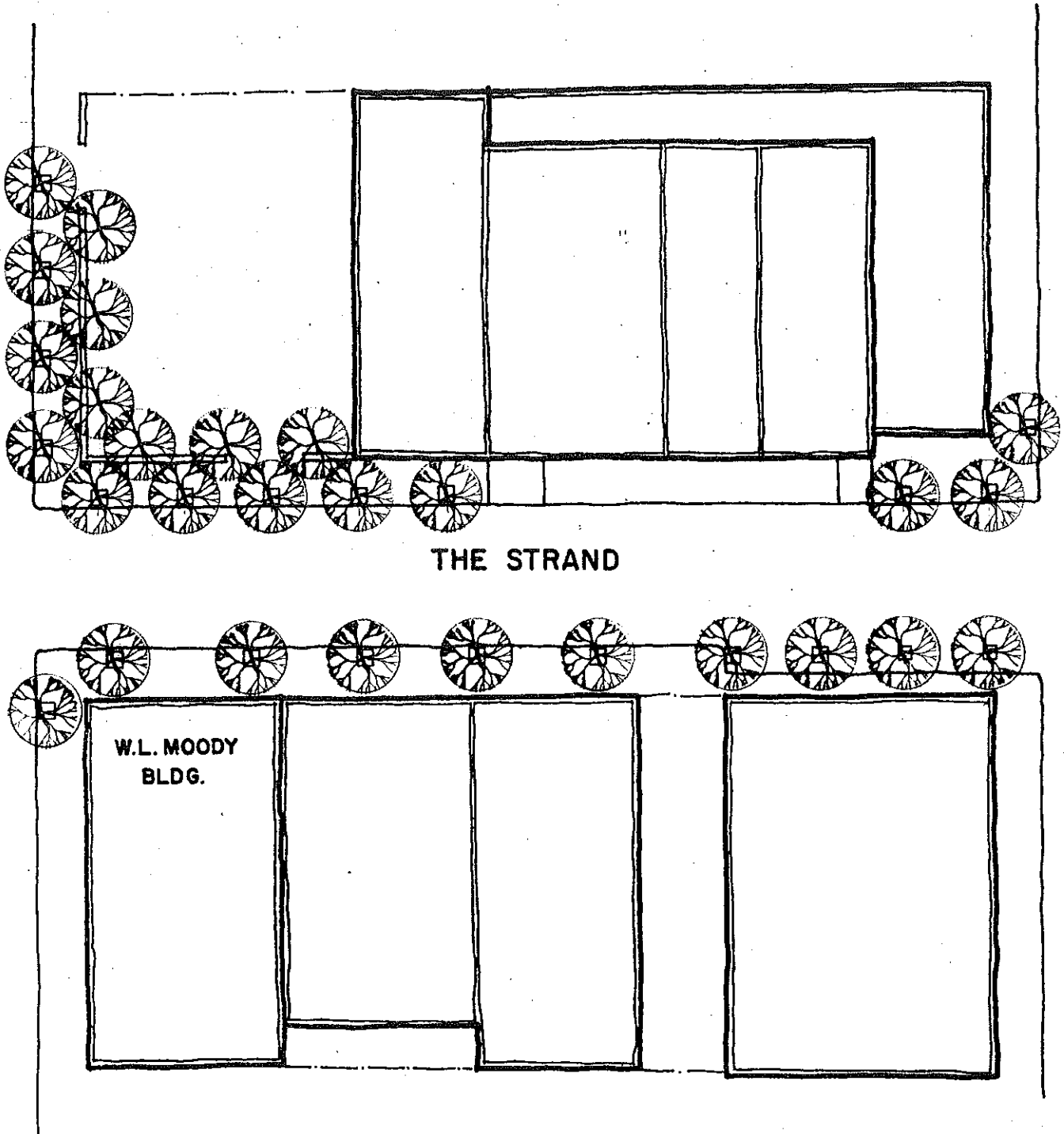
STRAND: LANDSCAPING
23RD TO 24TH

ALTERNATIVE 1

IX-13

22 ND STREET

23 RD STREET



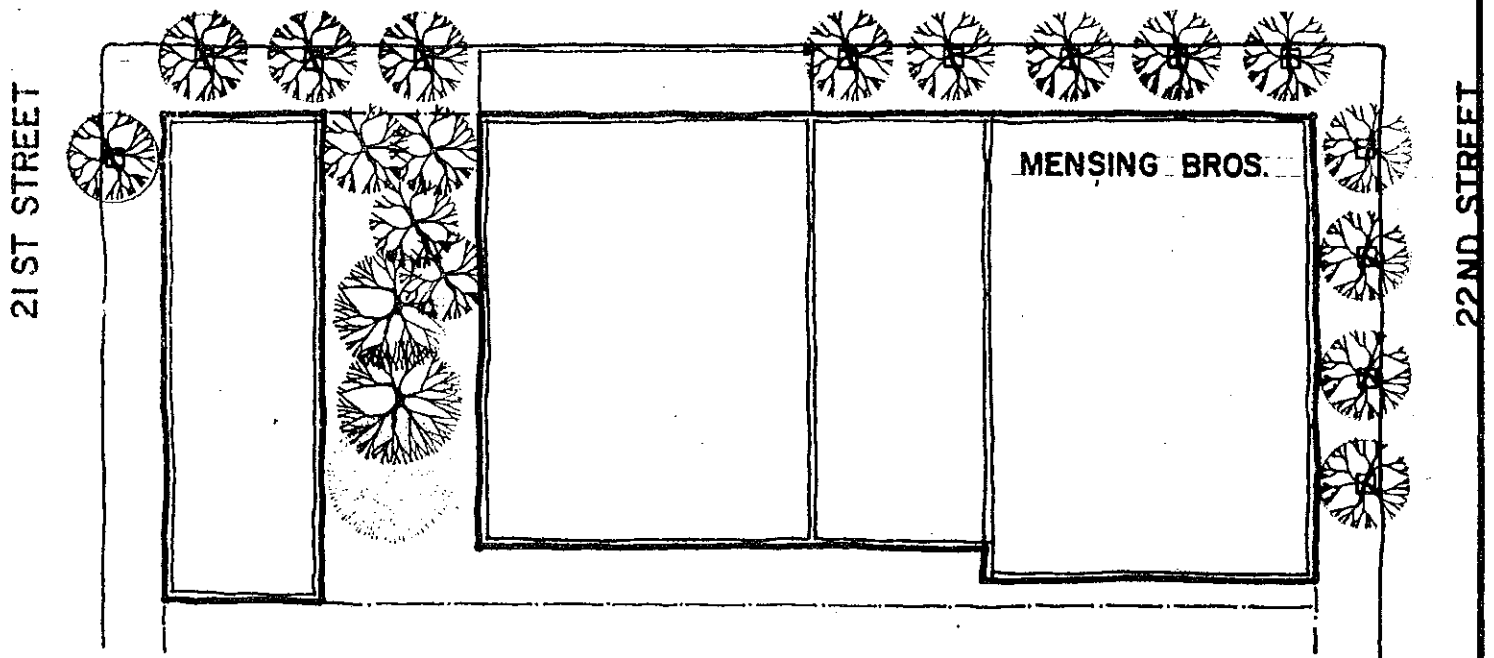
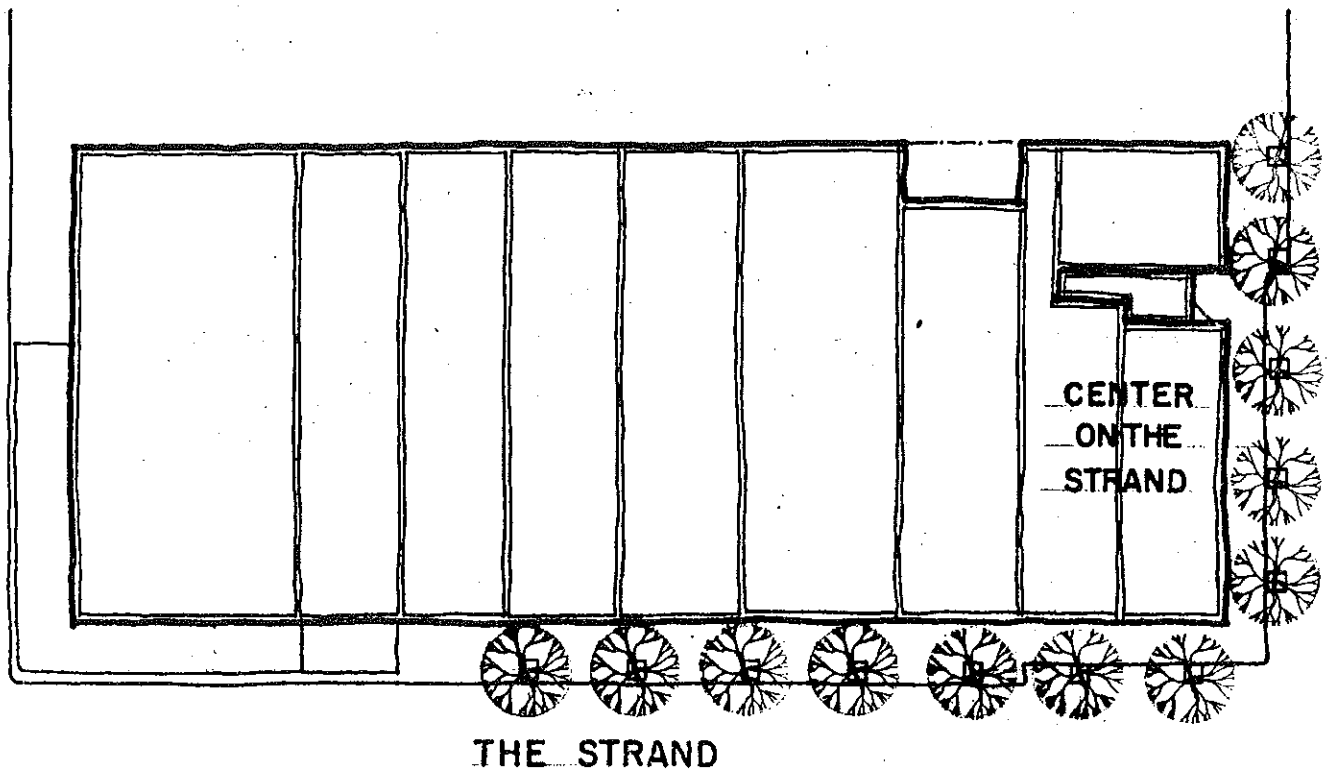
THE STRAND

W.L. MOODY
BLDG.

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STRAND: LANDSCAPING
22 ND TO 23 RD

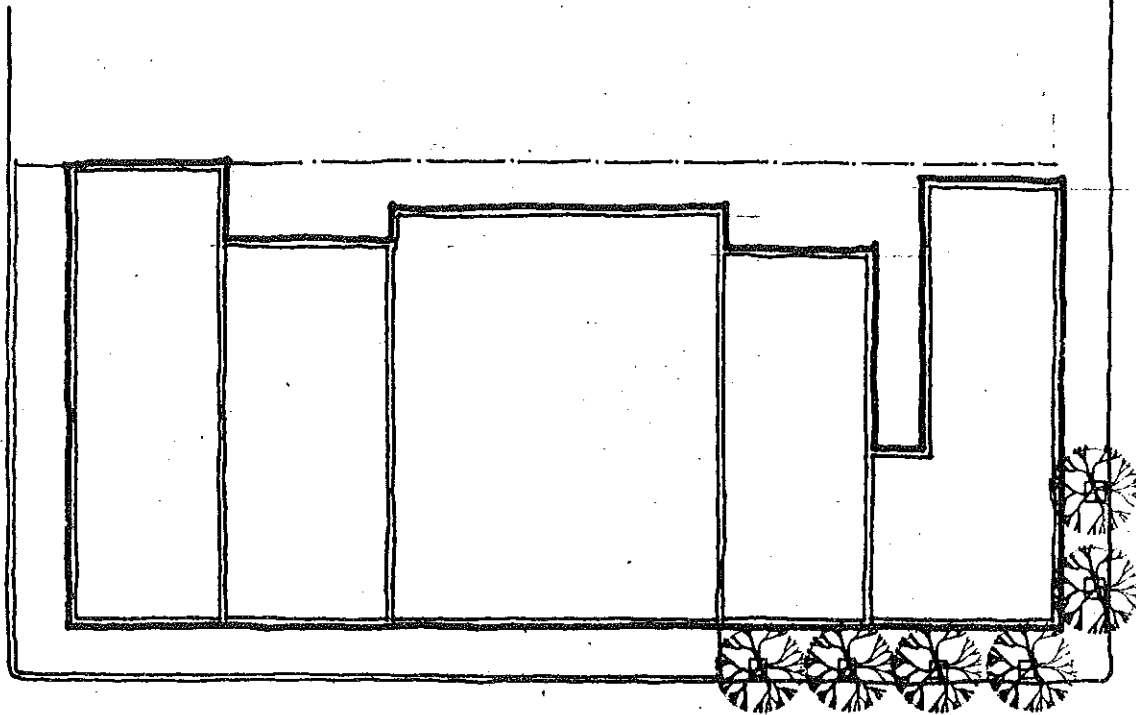
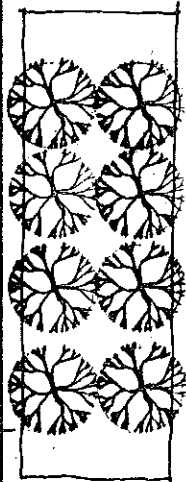
ALTERNATIVE I



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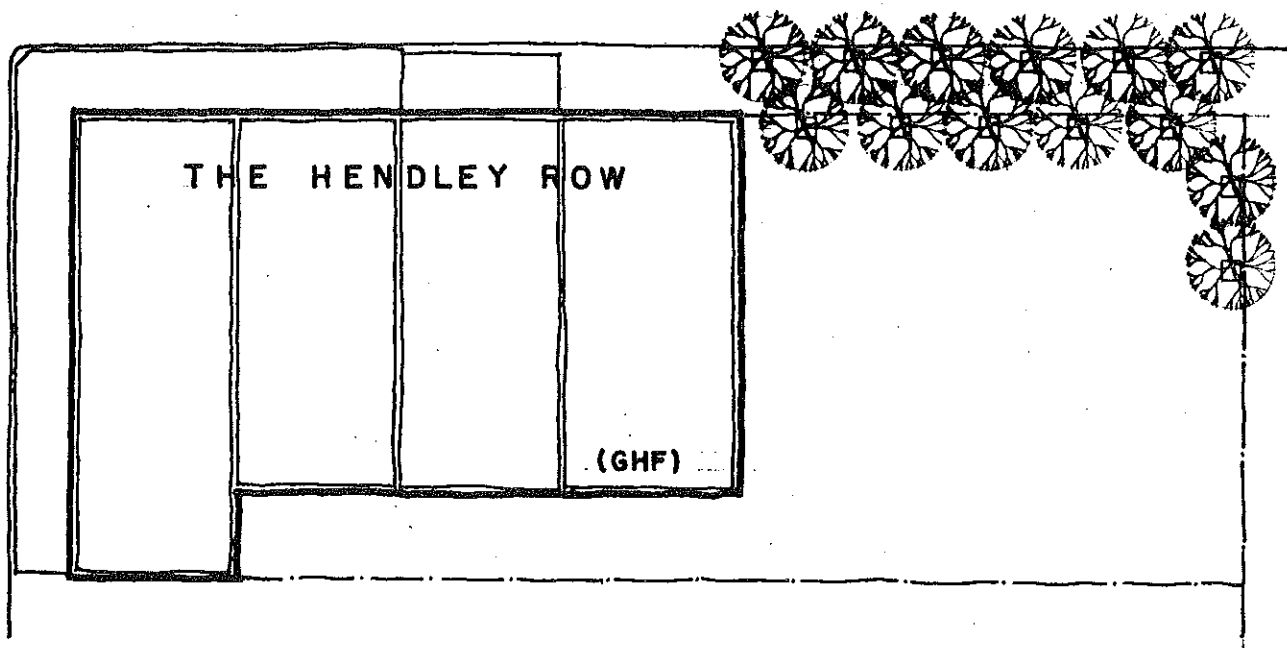
STRAND: LANDSCAPING
21ST TO 22ND

ALTERNATIVE I



THE STRAND

20 TH STREET



21ST STREET

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STRAND: LANDSCAPING
20 TH TO 21 ST

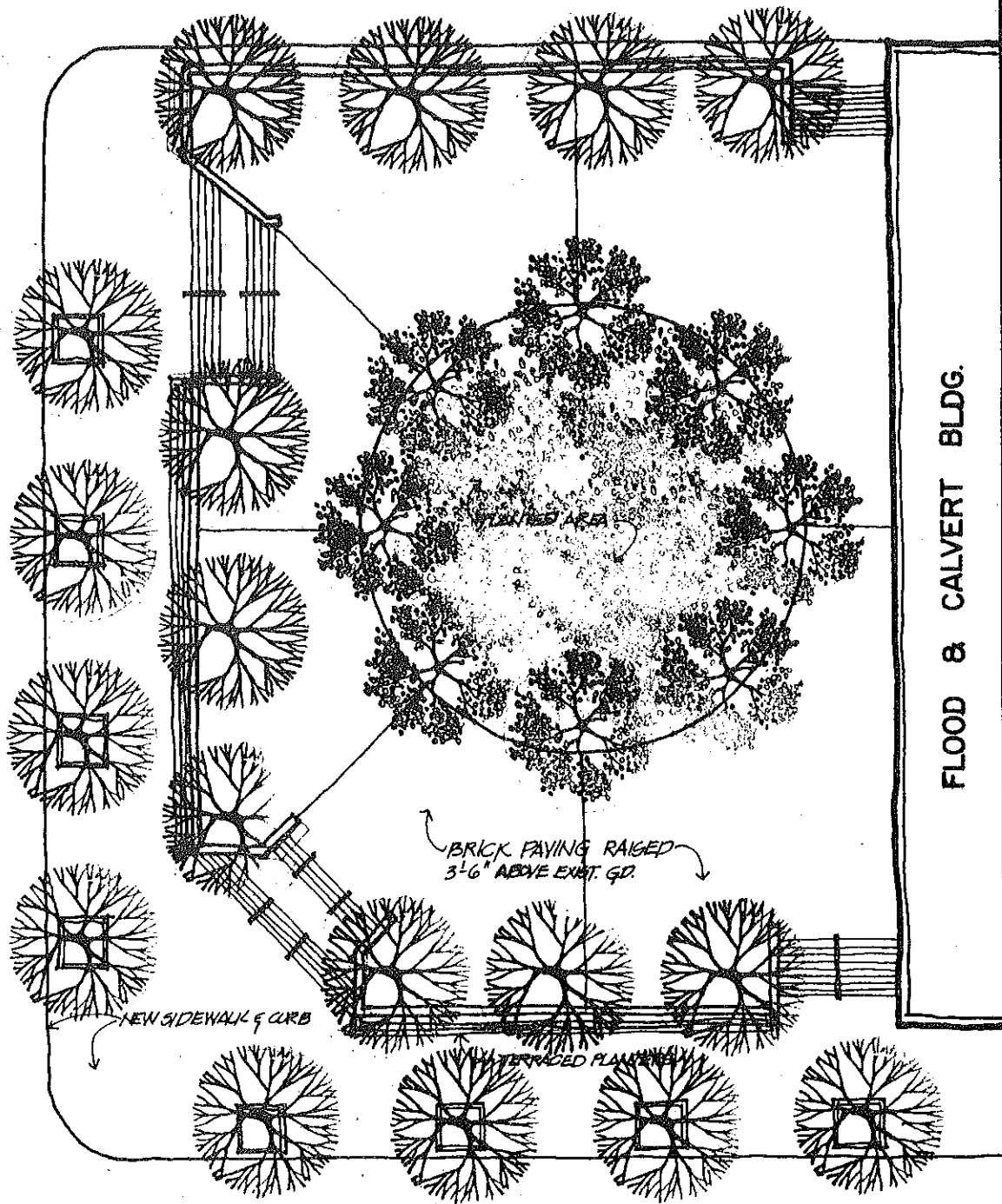
ALTERNATIVE 1

IX-16

ART. GALLERY &
GALVESTON HISTORICAL FOUNDATION
OFFICES

CENTER ON THE STRAND

22 ND



STRAND

FORD
POWELL &
CARSON
ARCHITECTS
& PLANNERS

STRAND: LANDSCAPING
PROPOSED PARK 22ND & STRAND

EXHIBIT 14, thru 17

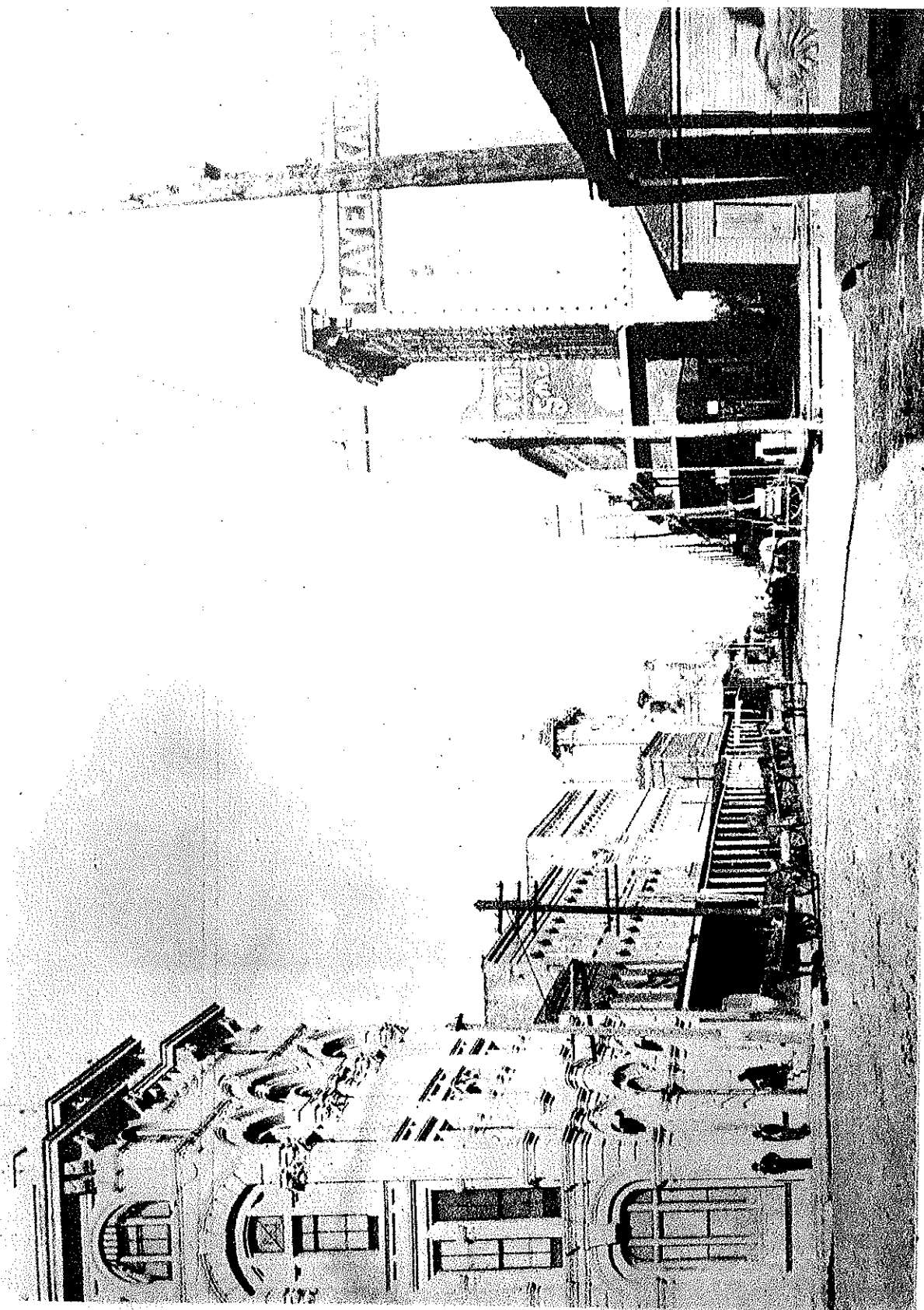


EXHIBIT 14: THE STRAND LOOKING EAST FROM 25TH, CIRCA 1882

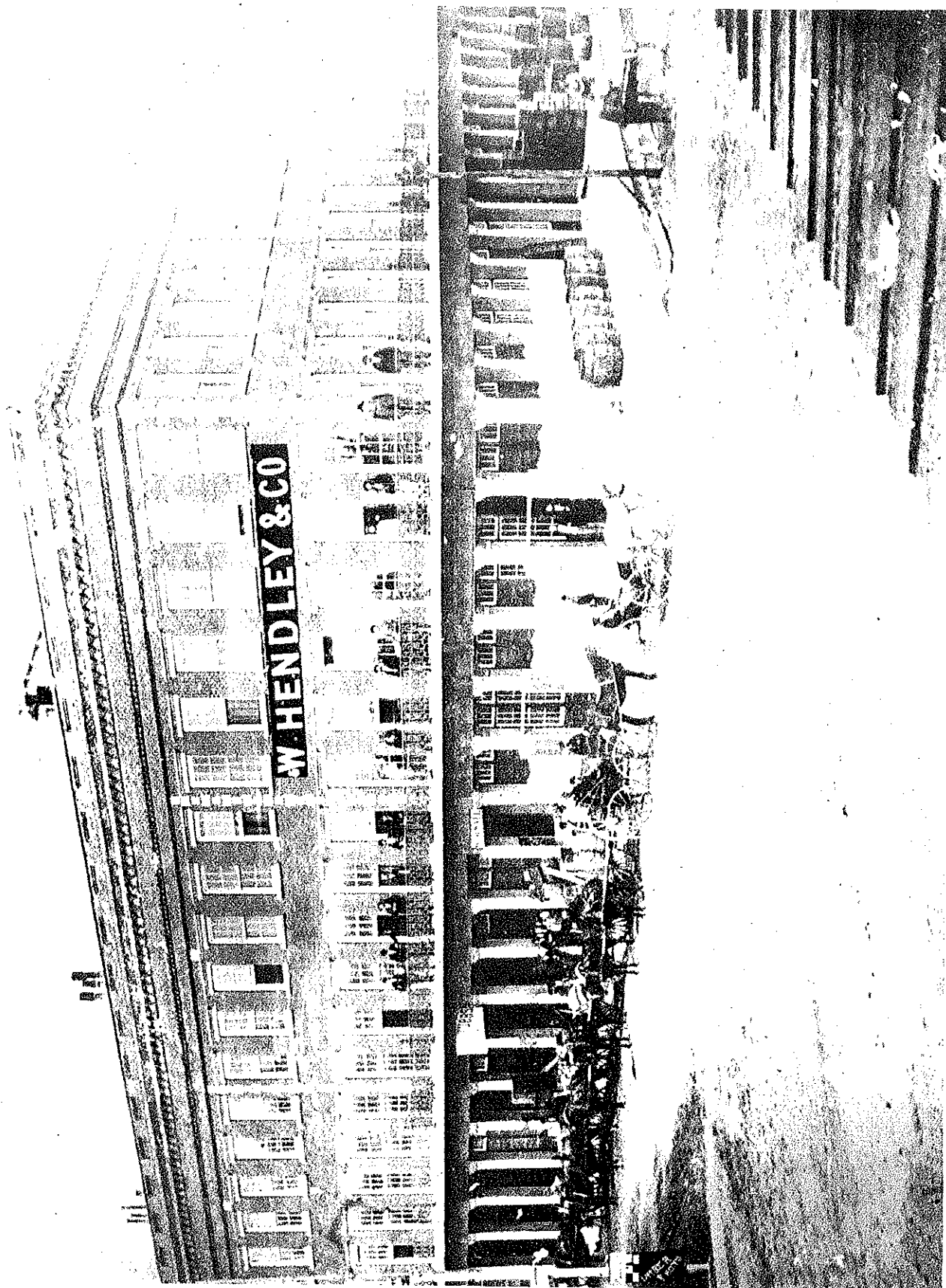


EXHIBIT 15: THE HENLEY ROW, STRAND AND 20TH, CIRCA 1868

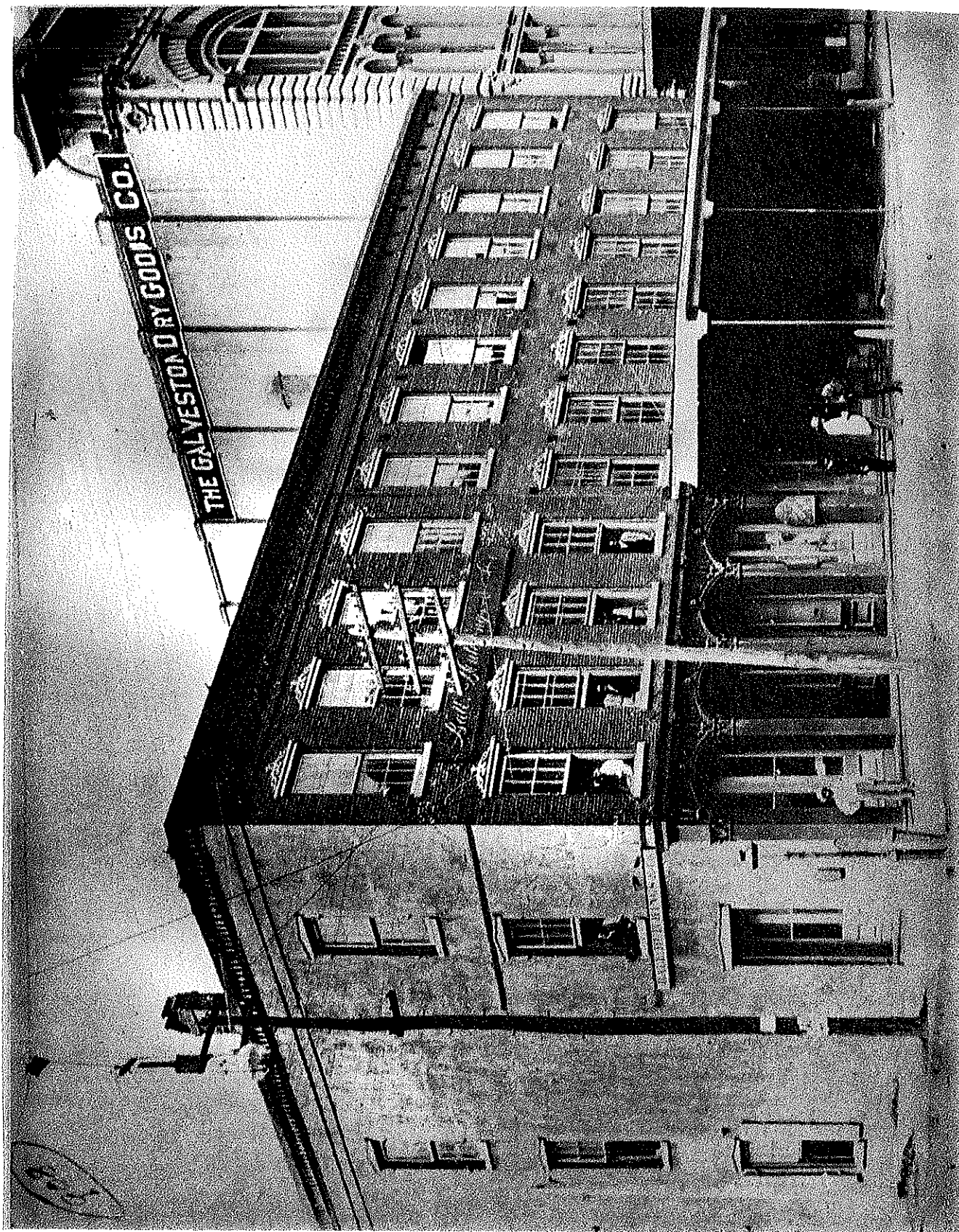


EXHIBIT 16: BALL HUTCHINGS AND CO., STRAND AND 24TH, CIRCA 1882

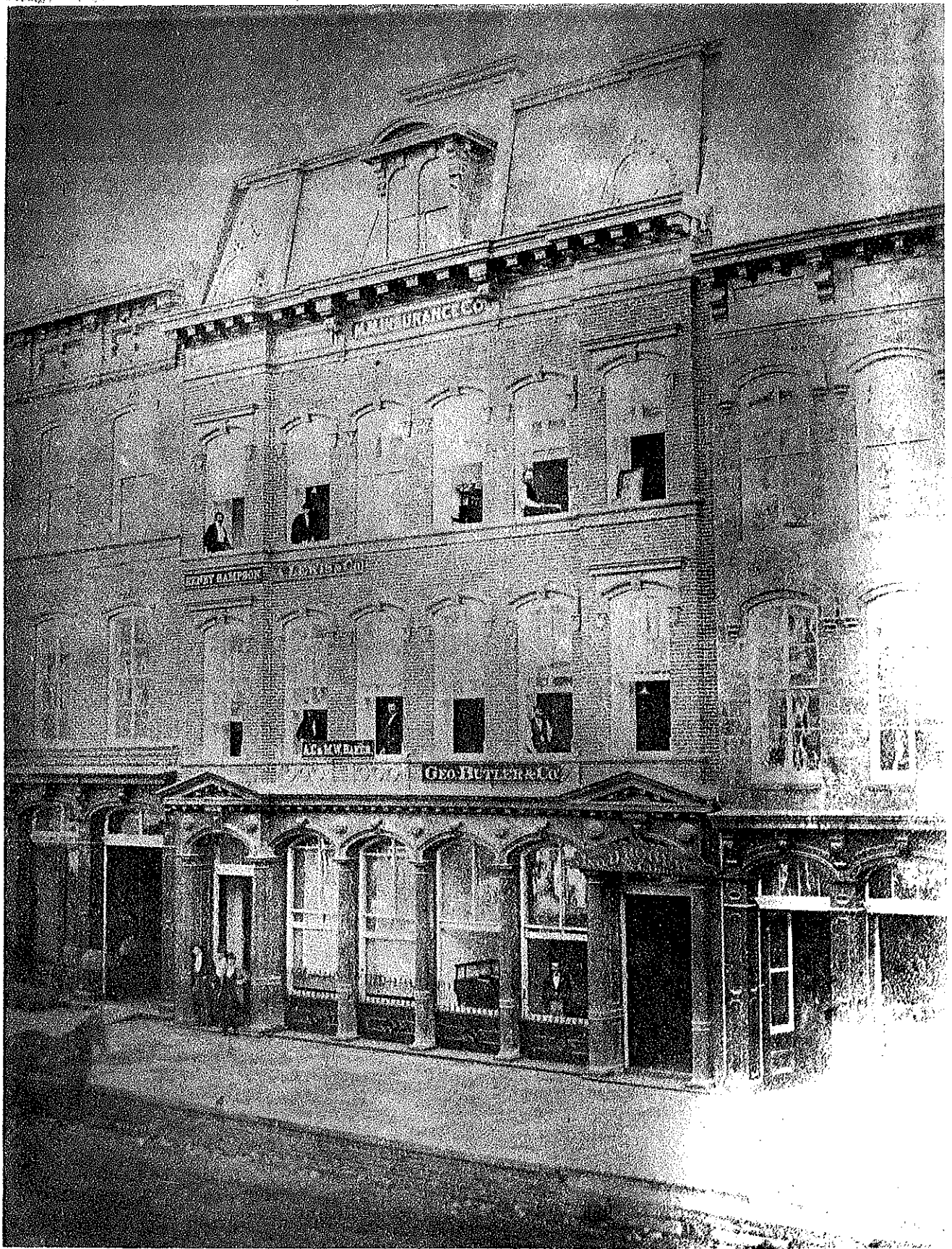


EXHIBIT 17 : MERCHANTS MUTUAL INSURANCE CO. BLDG., 2317-19 STRAND, CIRCA 1872

STRAND
23RD

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TYPICAL STREET SIGNS
GALVESTON STRAND

VII-13

EXHIBIT #10- PORCELAIN-ENAMEL SIGNS with silk-screened letters and symbols:

This category includes signs intended for installation outdoors where weather-resistant properties are required.

PANEL MATERIAL

Steel enameling stock 16 gage as approved and recommended by the Porcelain Enamel Institute for porcelain enamel application.

PANEL ERECTION

To be on existing poles or building walls as required by Architect.

LEGEND APPLICATION

Legends are to be applied from pattern in accordance with the standard grid by means of photographic silk-screen process at the shop. First-generation images from which patterns are to be produced shall be positive strip prints by photo-composition of 48-point characters of "phototypesetter" method of TradeType of Dallas, Inc., or a product of equal substance and function as approved by the Architect. Patterns are to be made to scale for each legend required, indicating letter and numeral forms, size, spacing, height, weight, wording, spelling, capitalization, punctuation, leading (interline spacing) and layout or composition.

COLOR SCHEDULE

Street Signs

Panel	-	Slate grey
Large letters	-	white
Small letters	-	red
Symbols	-	red

VIII. Galveston Strand

SIDEWALKS, CURBS AND PAVING

General Statements:

The sidewalks and curbs throughout The Strand (20th to 25th) are for the most part original with the existing buildings (1880). The typical sidewalk varied with the individual building as can be seen on the south side of the 2300 block of Strand. (See Exhibit 11, p. 73). The materials for sidewalks include cut stone, English tile, concrete and brick layed in a variety of patterns. The curbs were both brick and cut stone with the majority being brick having a curved top rolock course which was typical. (See Exhibit 12 and 13, P. 74 & 75). The existing brick curbs have been either painted or covered with a 1" concrete topping both of which have destroyed their original character.

Street paving in the area is generally asphalt over existing brick which probably dated from 1900. Original brick paving can be seen between Strand and Mechanic on both 24th and 21st Streets. The alleys throughout the Strand area have bricks that have ± 1" layer of asphalt. The asphalt here has not been maintained regularly and thus a large amount of brick is exposed. The alleys (particulary between Strand and Mechanic) could ultimately become pedestrian streets with shops occurring in the back of buildings, thus giving the potential for utilizing both ends of the typically long narrow buildings. The alleys are more in scale with pedestrian movement and the rear of the buildings provide the simple form and fine quality brick masonry that have been restored successfully in other cities. Service to these new pedestrian shops could be coordinated in off business hours. Existing service to buildings is primarily from Strand and side streets with the alleys now providing only limited service.

Recommendations:

A survey has been made of existing conditons for sidewalks, curbs and paving.(see drawings 5 thru 9,pp. 61 to65). From these existing conditons, plans have been drawn to illustrate specific areas for renovation or reconstruction. (See drawings 10 thru 14,pp. 67 to 71).

Owners along Strand should be encouraged with assistance from the city to engage in this sidewalk and curb restoration with examples and standards furnished by the Historical Foundation.

Selected Sidewalk Materials:

Many alternatives in material and shape would be in character with Strand and the suggested alternatives were developed from the existing Strand sidewalk materials.

1. Brick paving on uncompacted asphalt bed - cost \pm \$1.65/SF. A wide variety of patterns and colors can be achieved using brick.
2. Unglazed tile 12" x 12" on uncompacted asphalt bed - cost \pm \$2.50/sf.
This paving is laid in a diagonal pattern and used at the First National Bank and Trueheart-Adriene Building.
3. Sandstone and slate pavers - cost \pm \$2.65/sf.

Curbs and Pavings:

For the most part, all the original brick curbs exist but have been covered with 1" concrete topping that is now chipping and breaking away. Restoration of curbs would require removing this concrete topping, cleaning and repointing all brick. New curbs can be built to match the existing condition or provide a similar shape but with a new material.

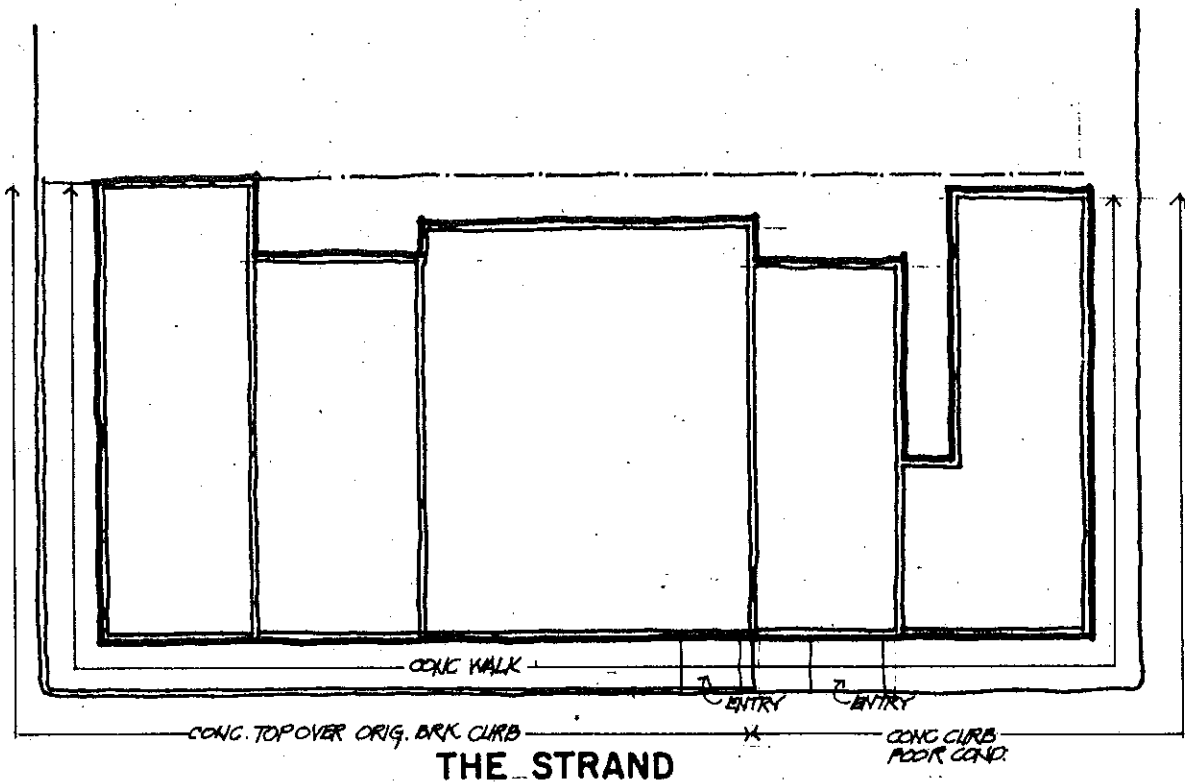
All traffic control paint should be removed from all curbs and traffic should be controlled by new international signs.

All streets and alleys should be restored to their original brick when possible. While this might be difficult for Strand with its heavy traffic load, cross streets (20th thru 25th) and alleys offer the potential for this to occur, however, total restoration is a long range goal.

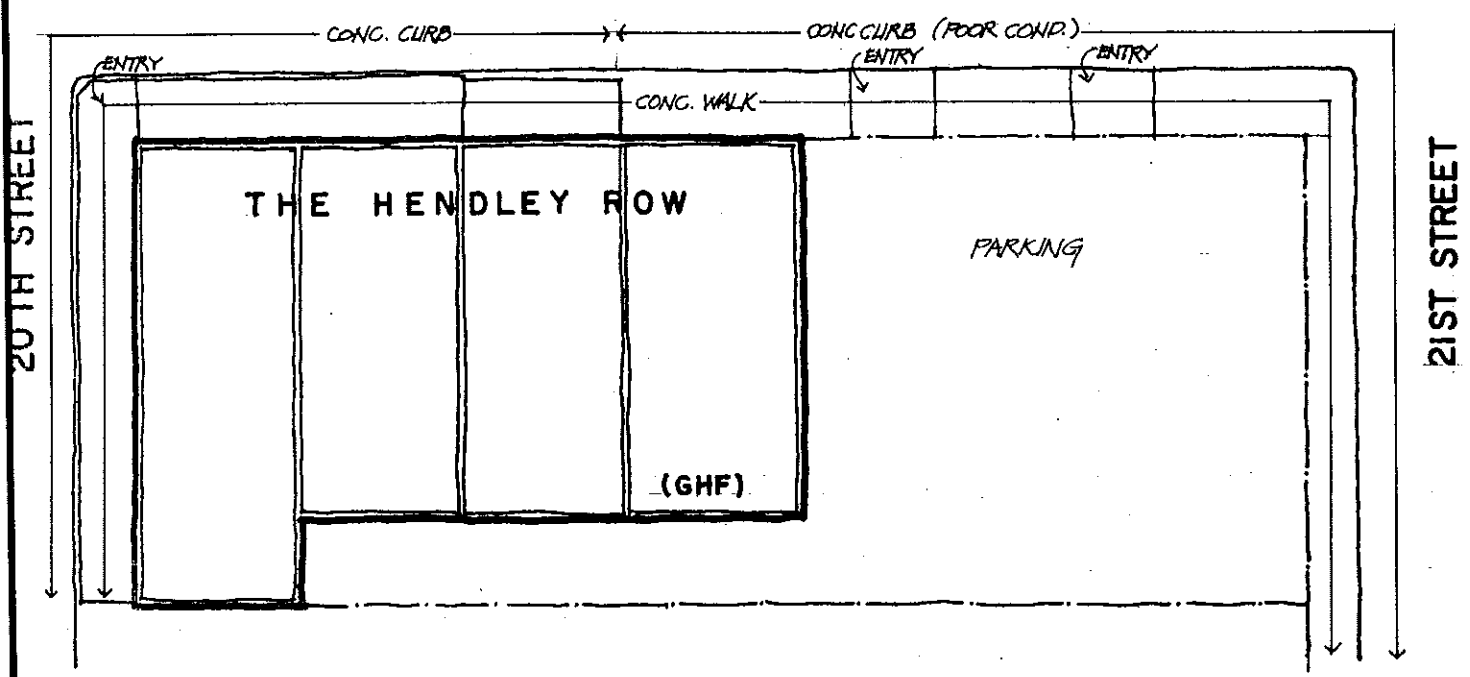
Repair/renovate existing brick curbs

remove topping	\$1.00/sf
sand blast brick	.35/lf
repoint brick	2.25/lf

Drawings 5 thru 9



THE STRAND

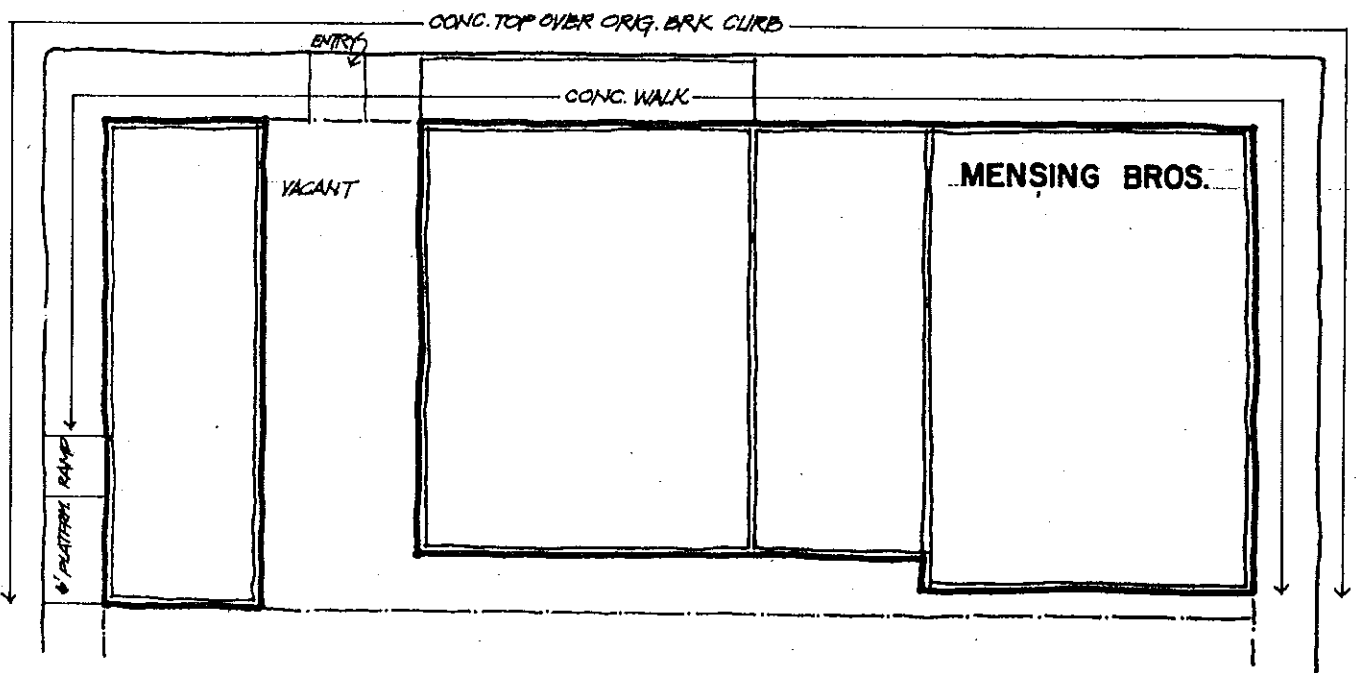
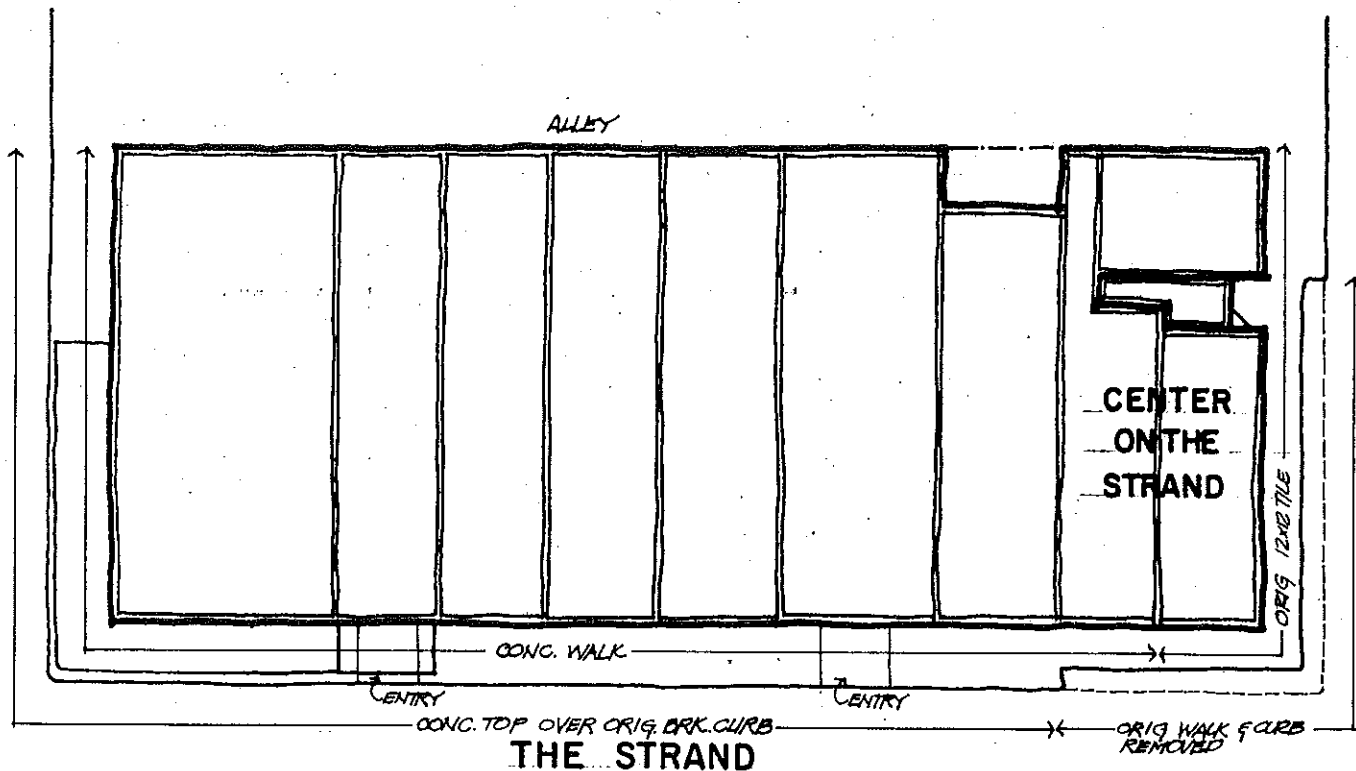


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**STRAND: SIDEWALKS, CURBS &
PAVING**

EXISTING CONDITIONS 20TH TO 21ST

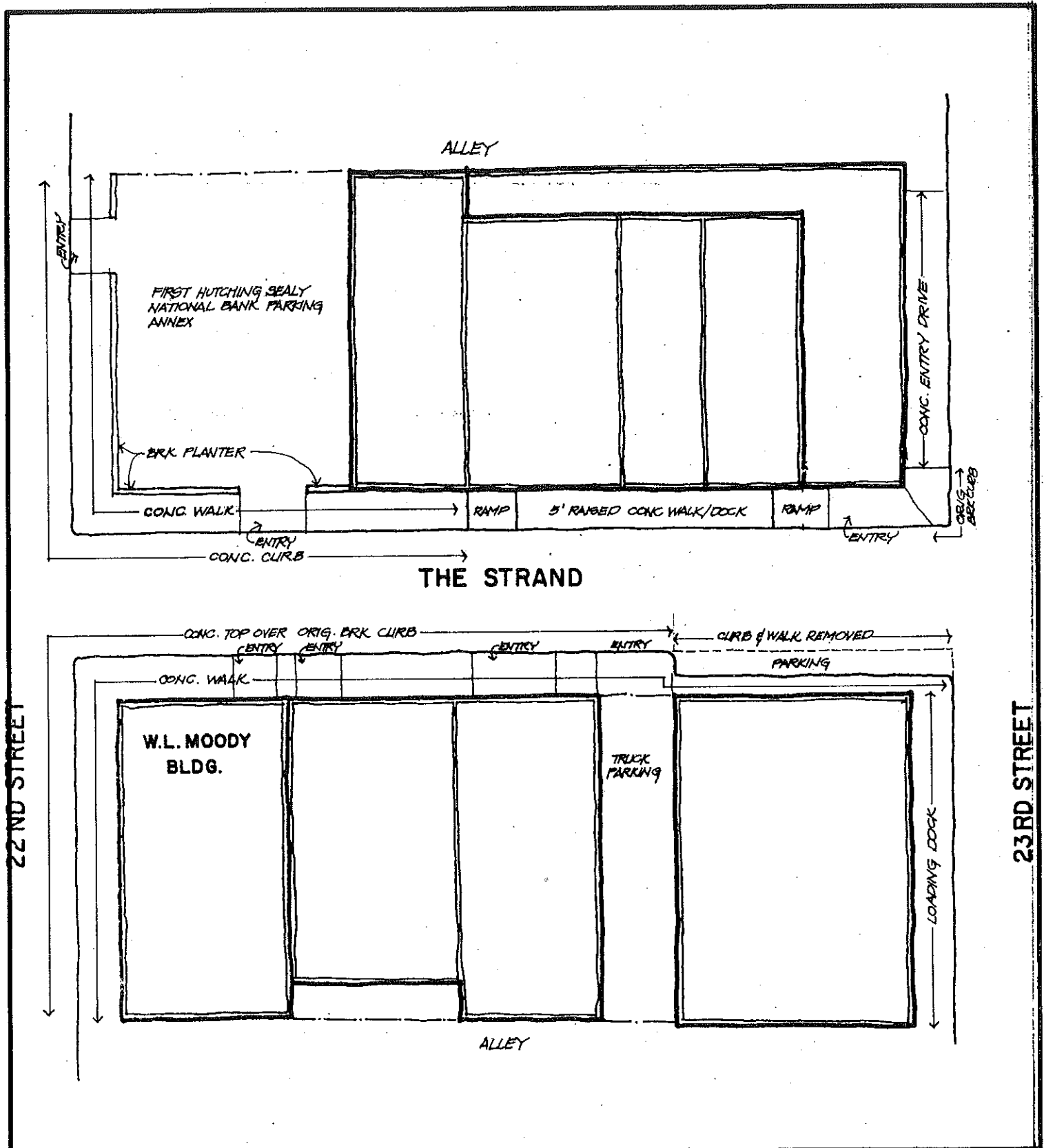
VIII-5



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STRAND: SIDEWALK, CURBS &
PAVING

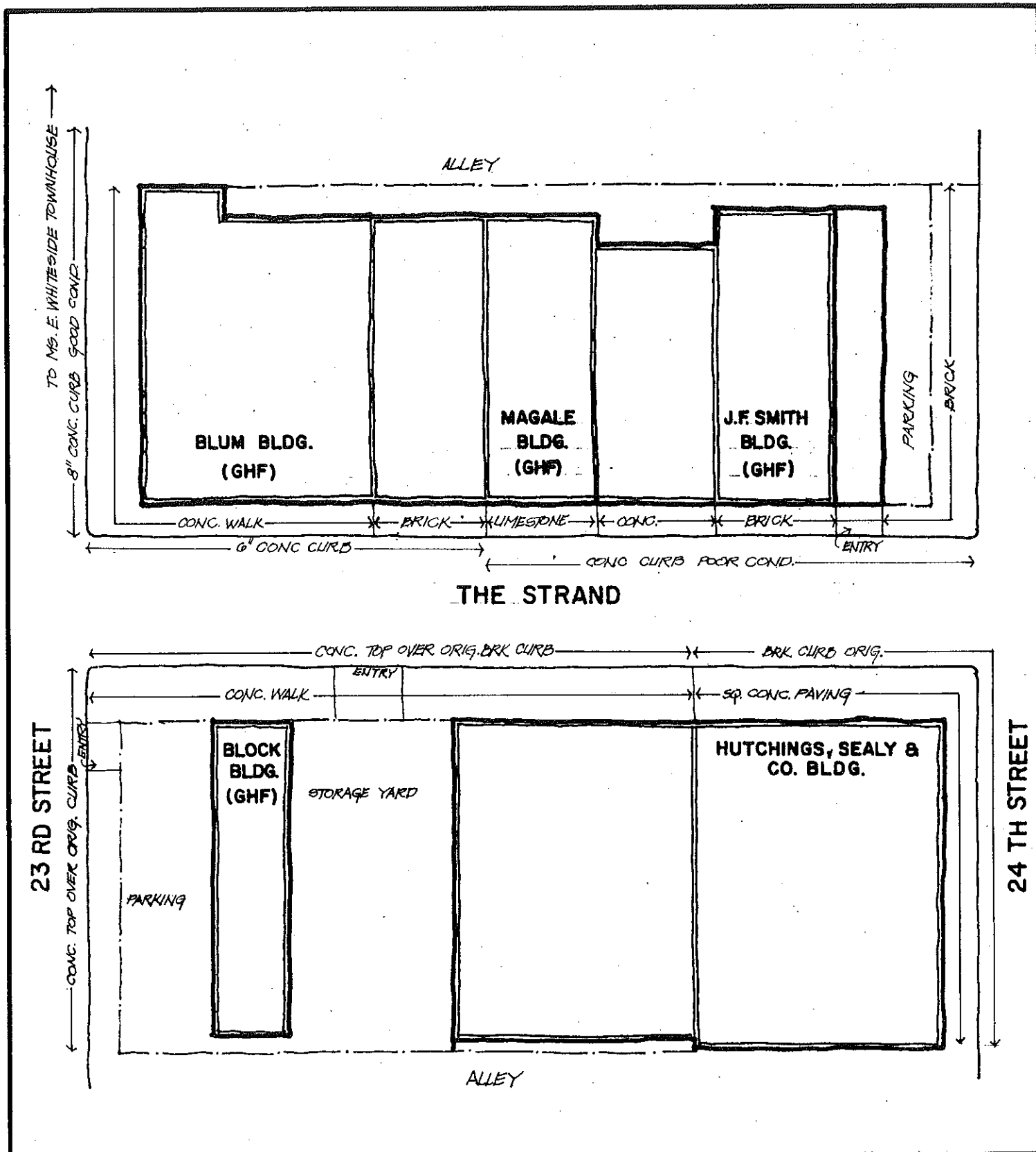
EXISTING CONDITIONS 21ST TO 22ND



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STRAND: SIDEWALKS, CURBS &
PAVING

EXISTING CONDITIONS 22ND TO 23RD



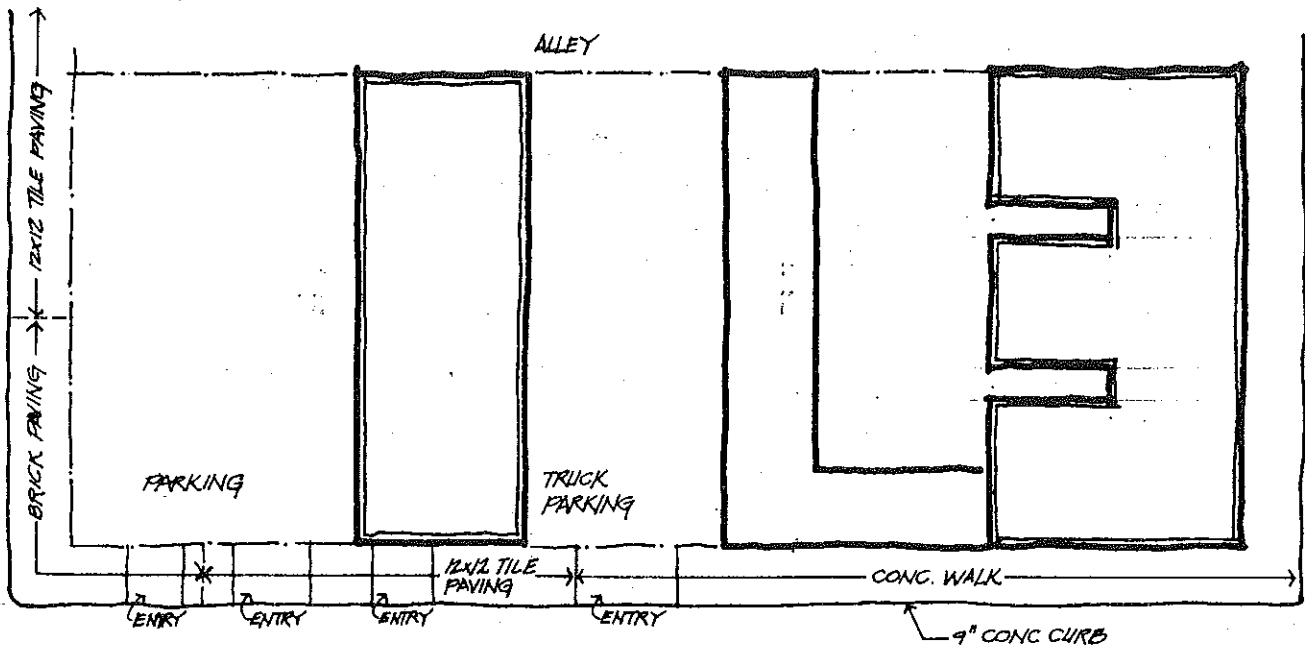
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STRAND: SIDEWALKS, CURBS &
PAVING

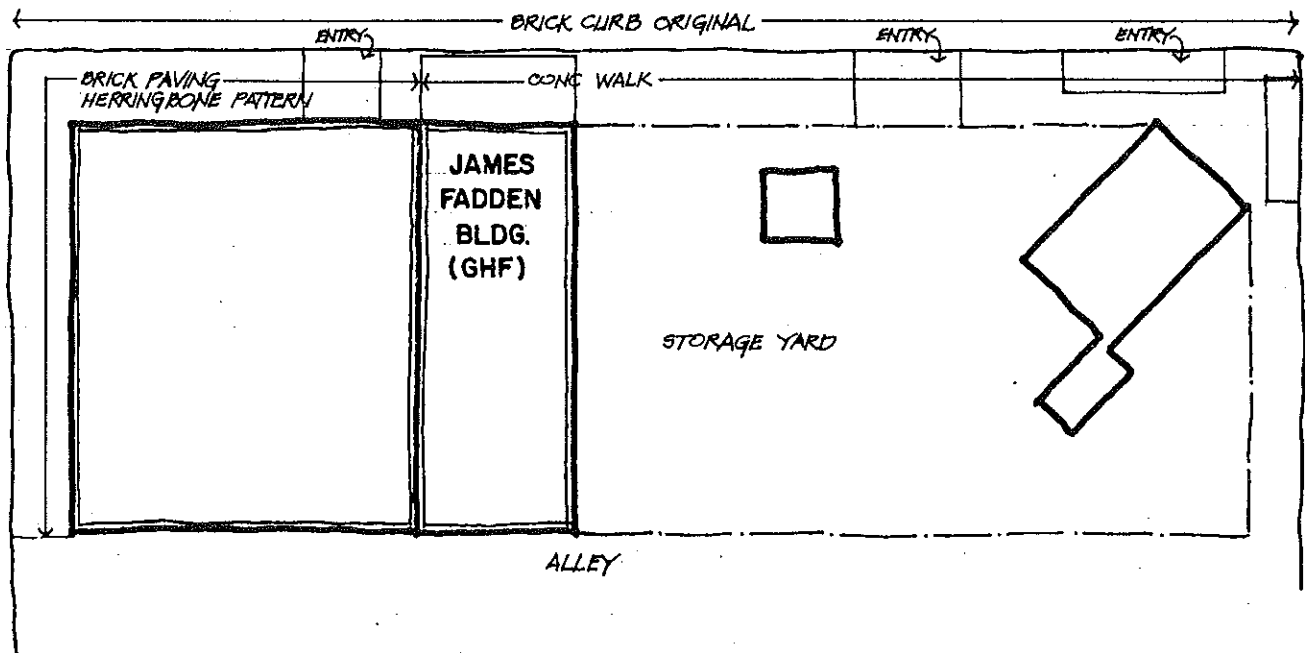
EXISTING CONDITIONS 23 RD TO 24TH

VIII-8



THE STRAND

24 TH STREET



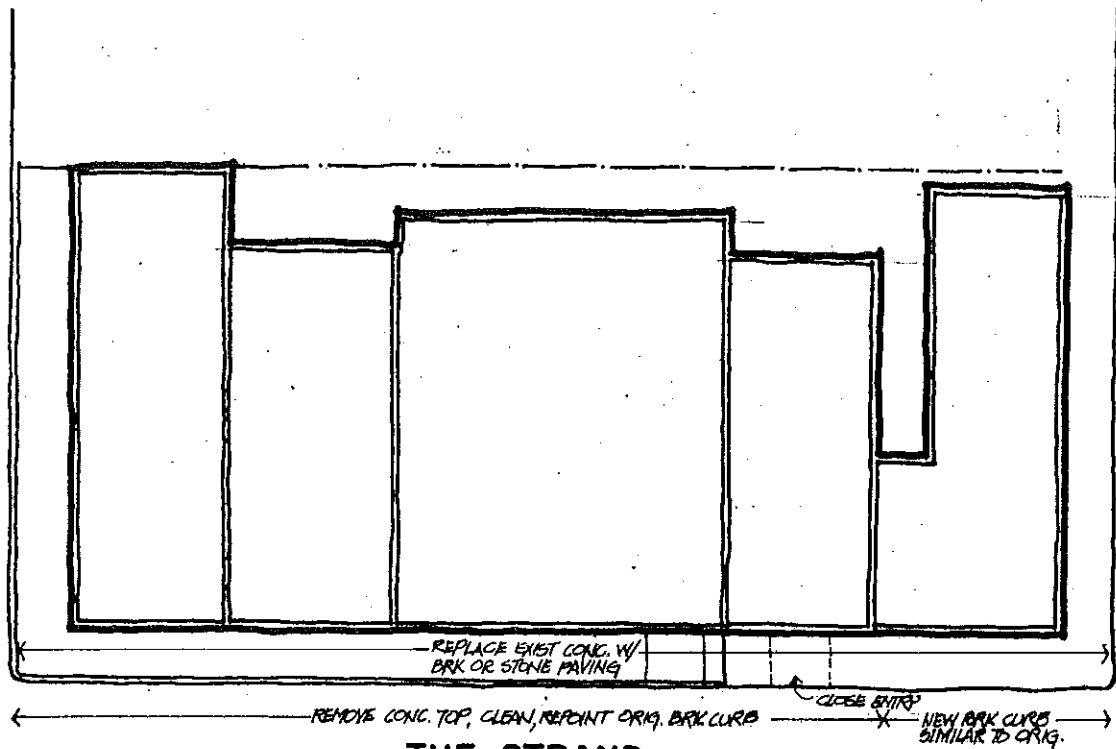
25 TH STREET

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CARSON
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& PLANNERS

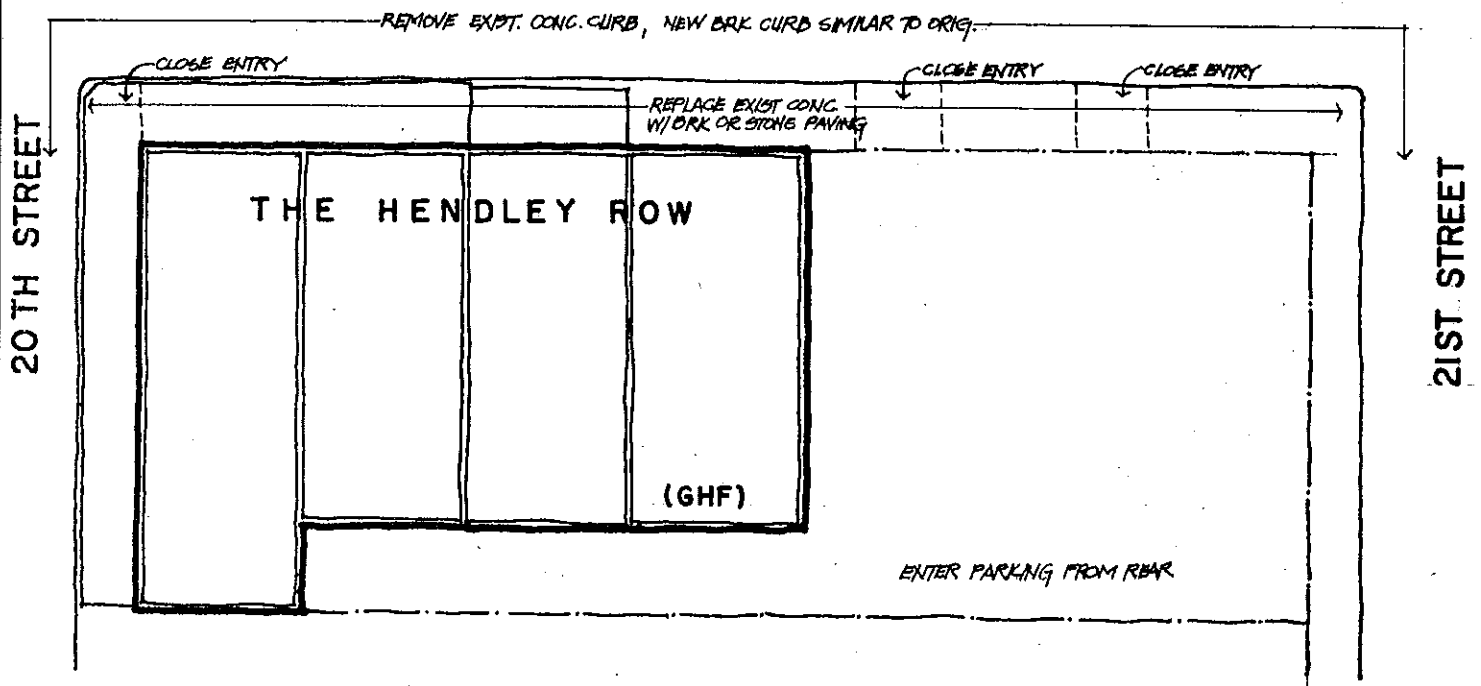
STRAND: SIDEWALKS, CURBS &
PAVING

EXISTING CONDITIONS 24 TH TO 25 TH

Drawings 10 thru 14



THE STRAND



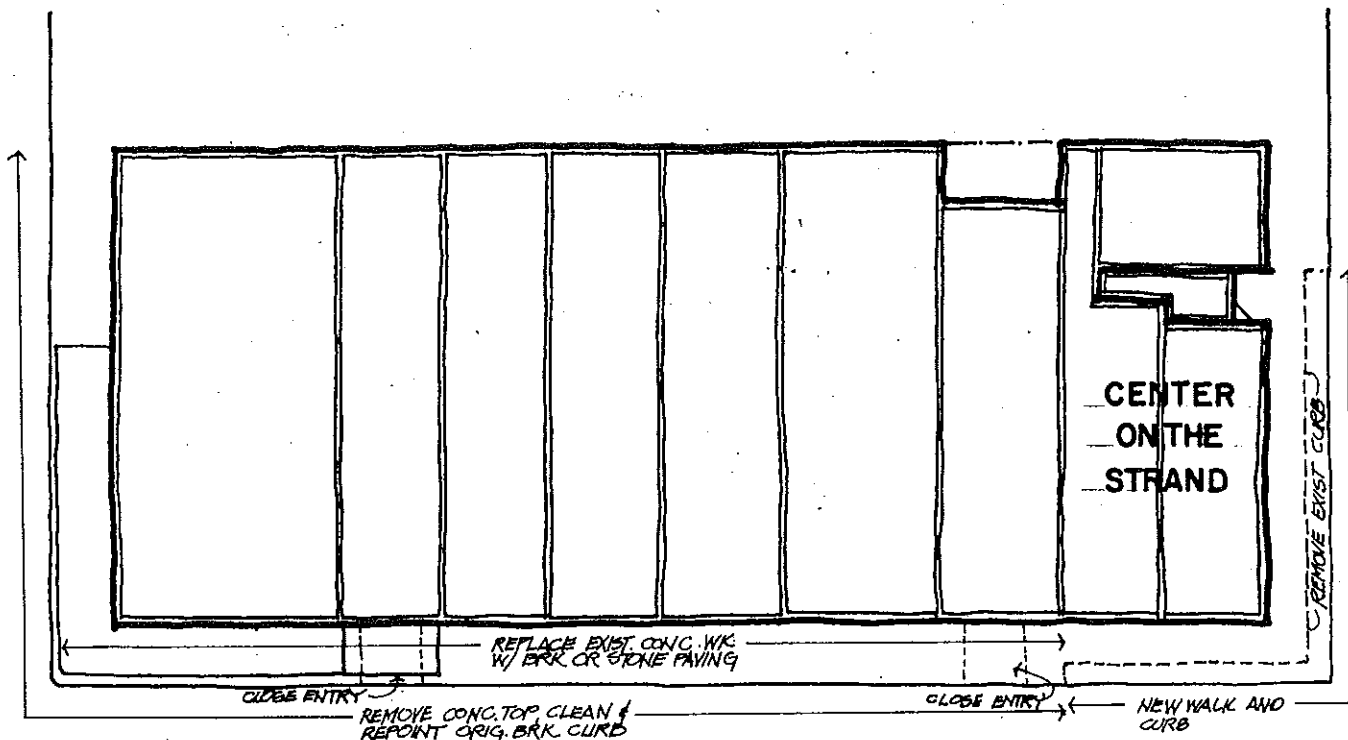
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STRAND: SIDEWALKS, CURBS &
PAVING

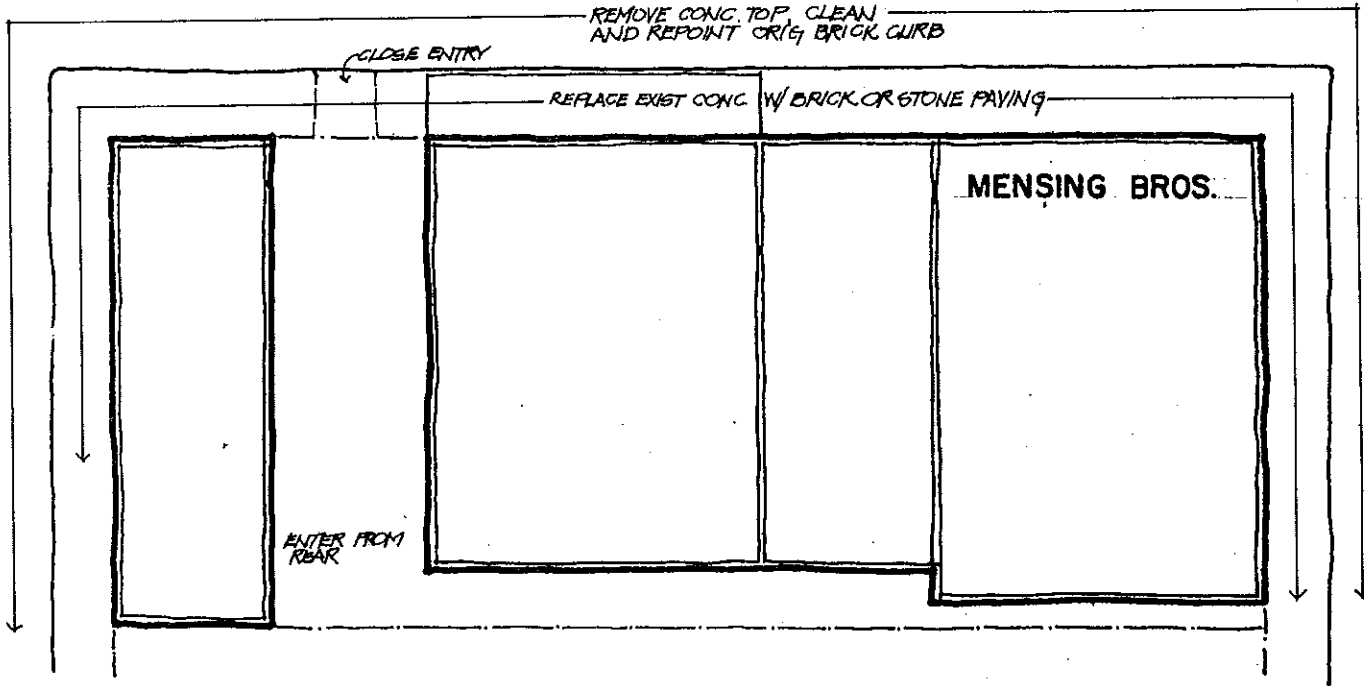
RESTORATION 20TH TO 21ST

VIII-11



THE STRAND

21ST STREET

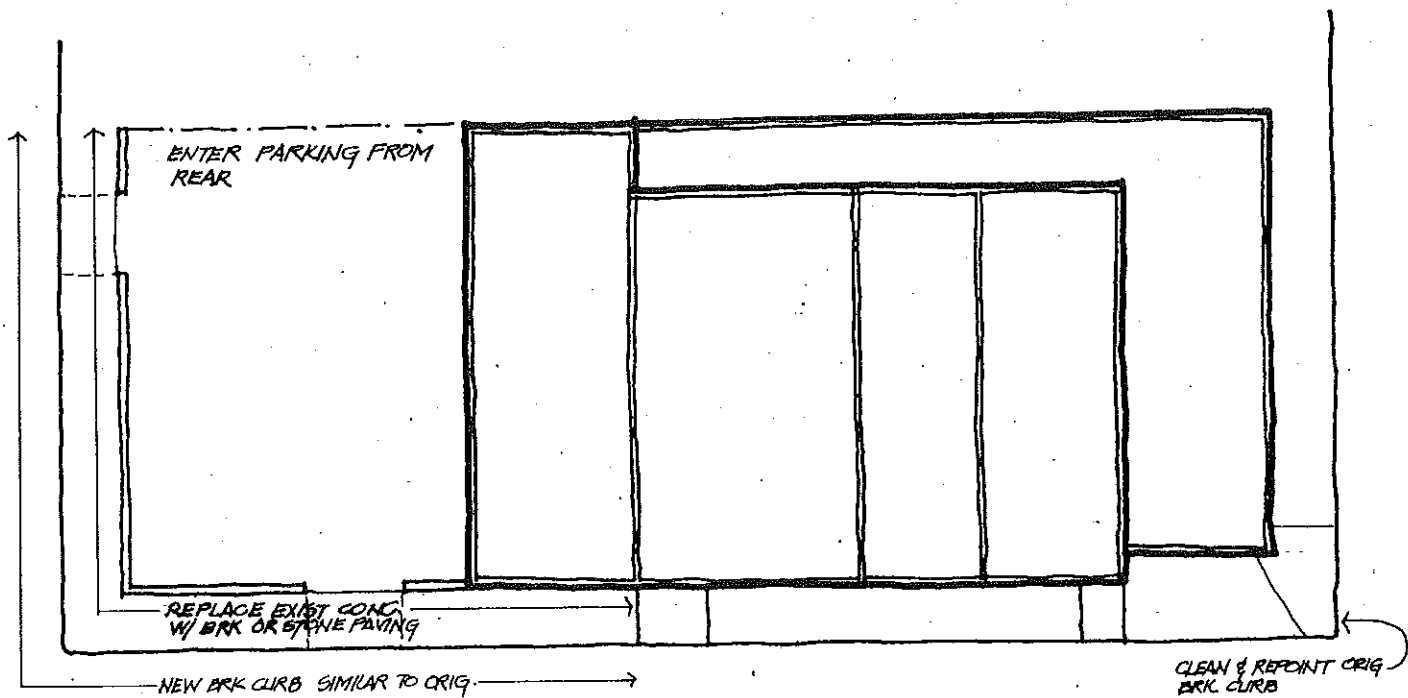


22ND STREET

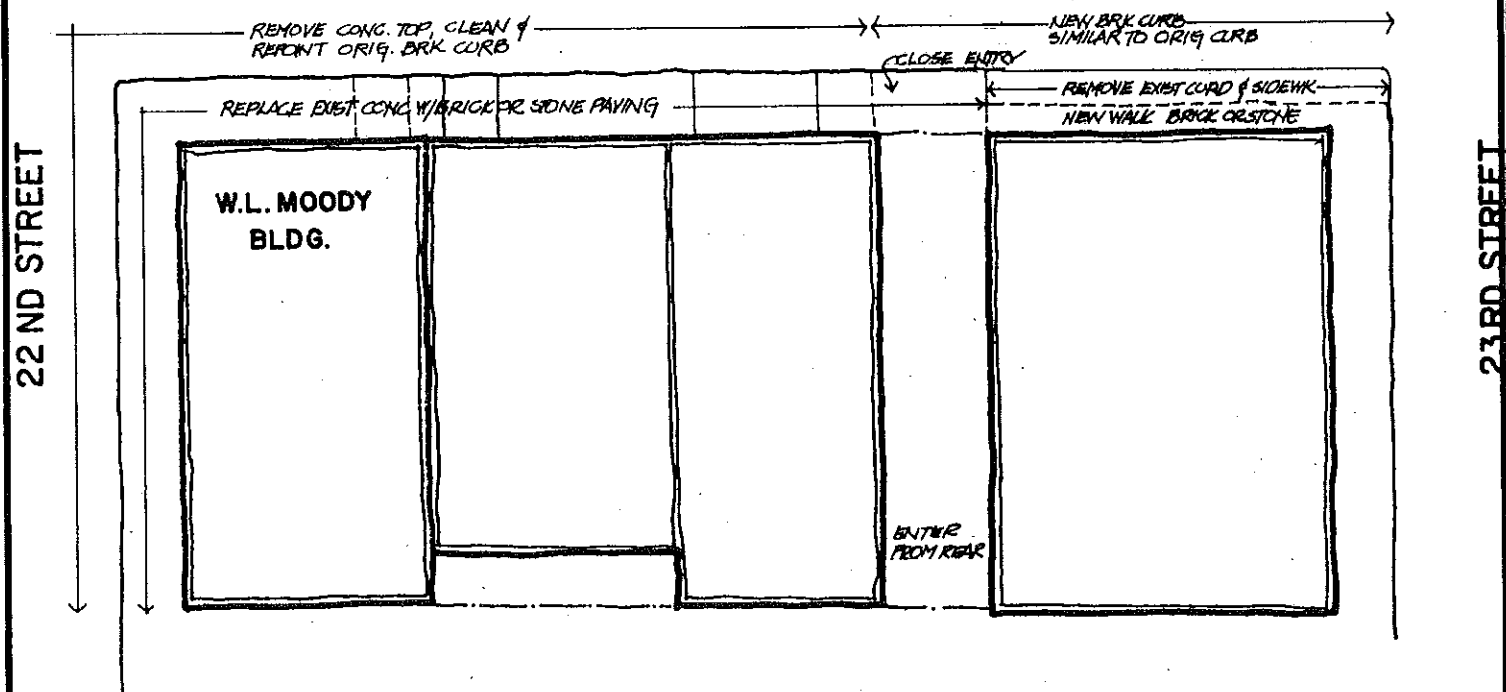
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**STRAND: SIDEWALKS, CURBS &
PAVING**

RESTORATION 21 ST TO 22 ND



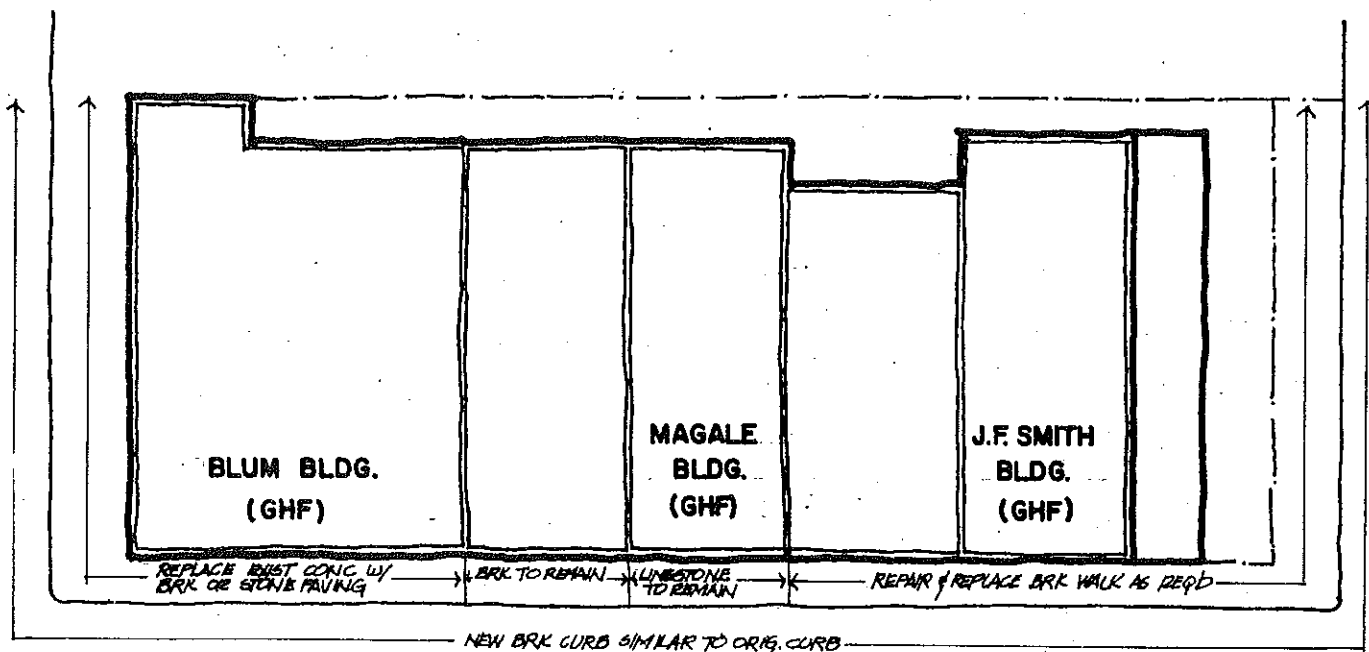
THE STRAND



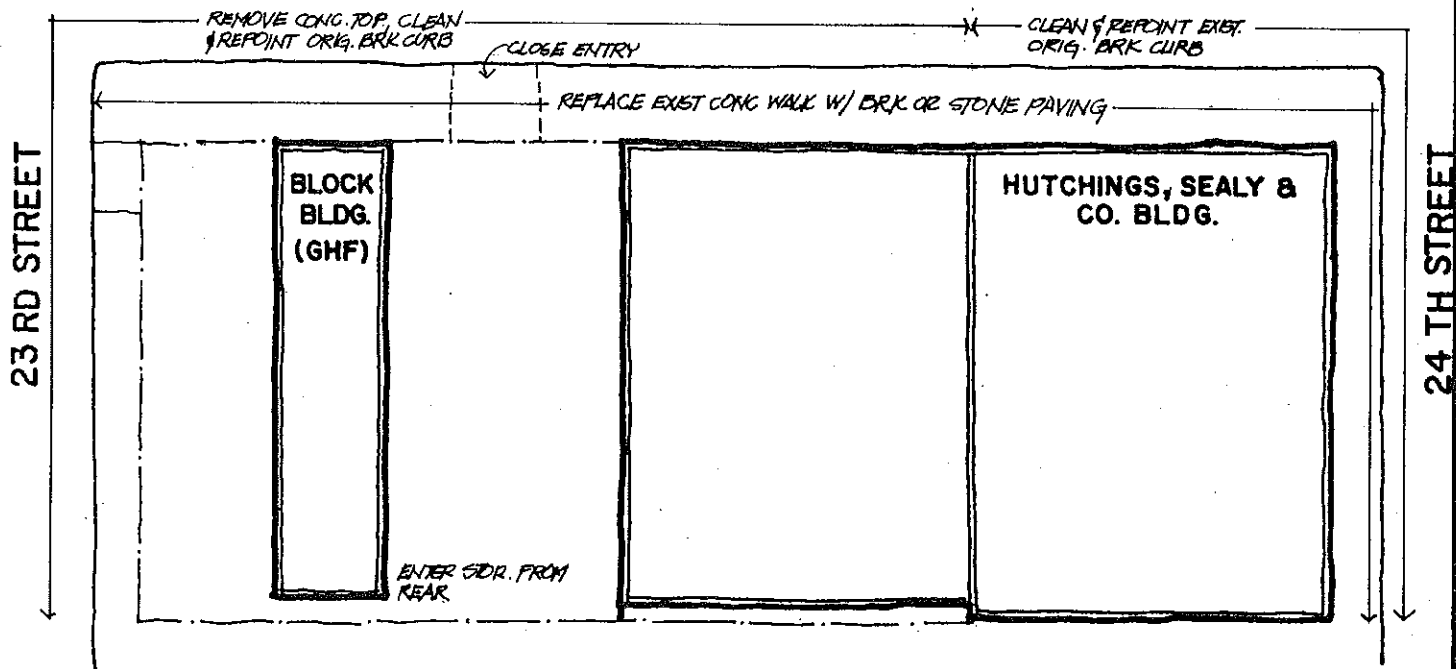
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STRAND: SIDEWALKS, CURBS &
PAVING

RESTORATION 22ND TO 23RD



THE STRAND



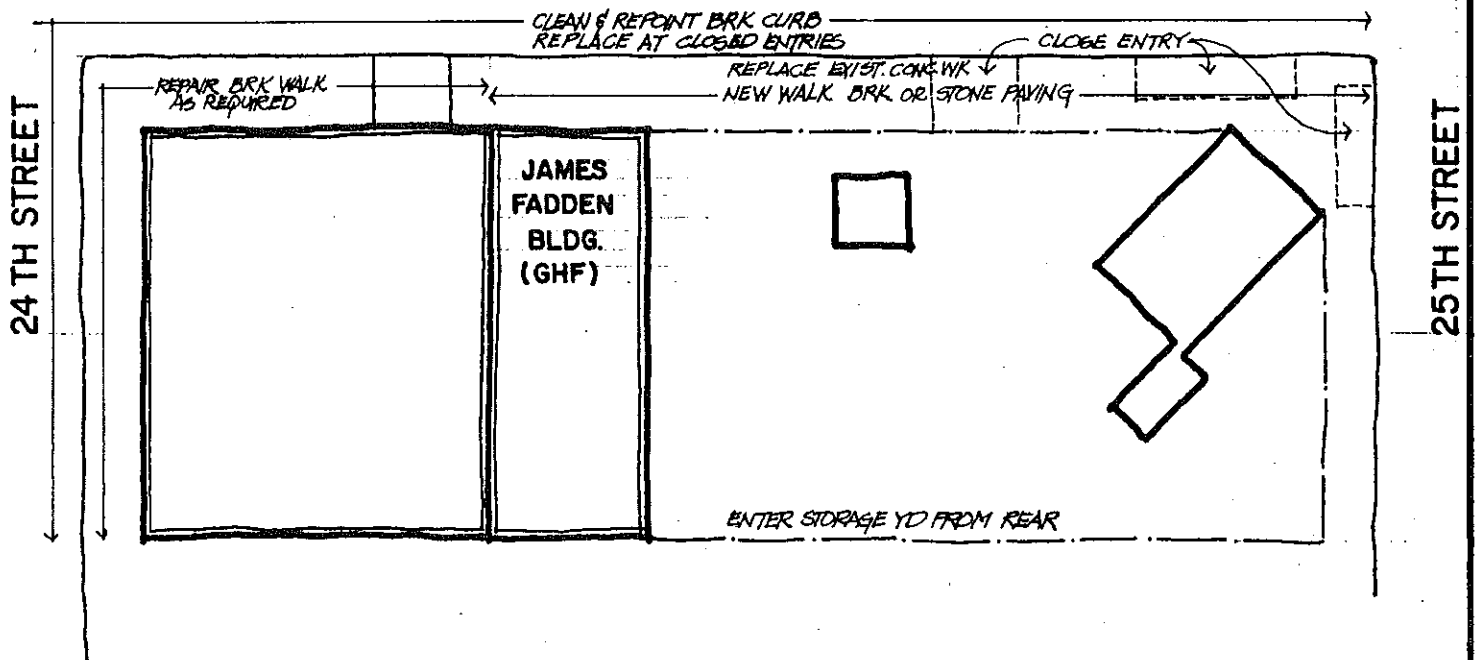
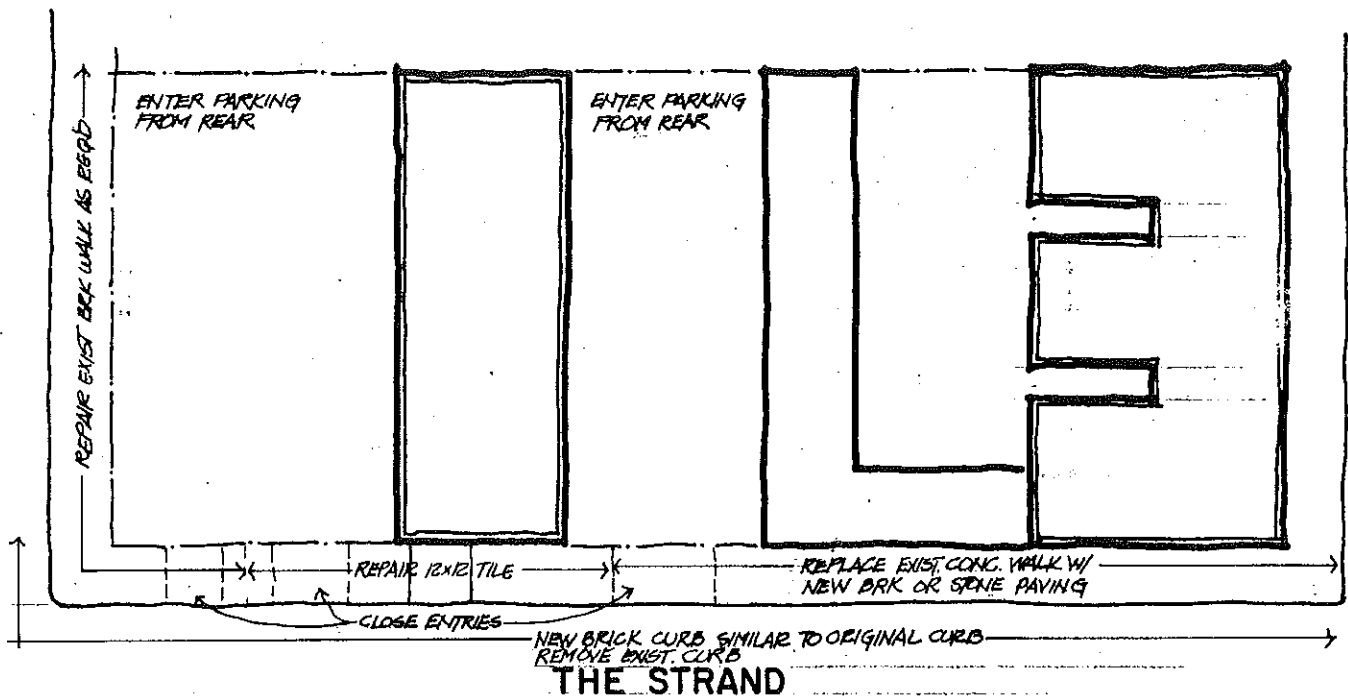
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**STRAND: SIDEWALKS, CURBS &
PAVING**

RESTORATION 23RD TO 24TH

VIII-14



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**STRAND: SIDEWALKS, CURBS &
PAVING**

RESTORATION 24 TH TO 25 TH

Exhibits 11 thru 13

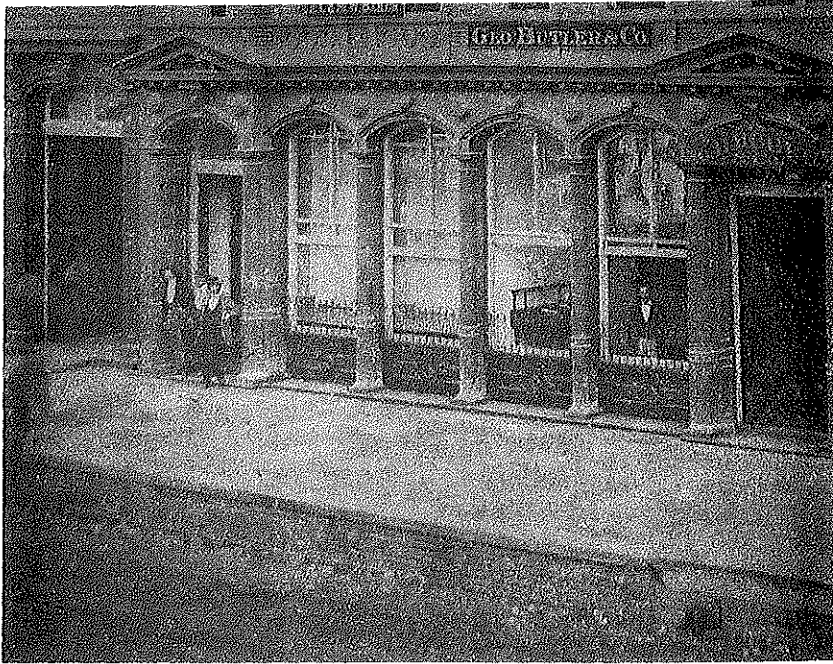


EXHIBIT 11 :

PAVING AT THE MERCHANTS

MUTUAL BUILDING

2317-19 STRAND

CIRCA 1875

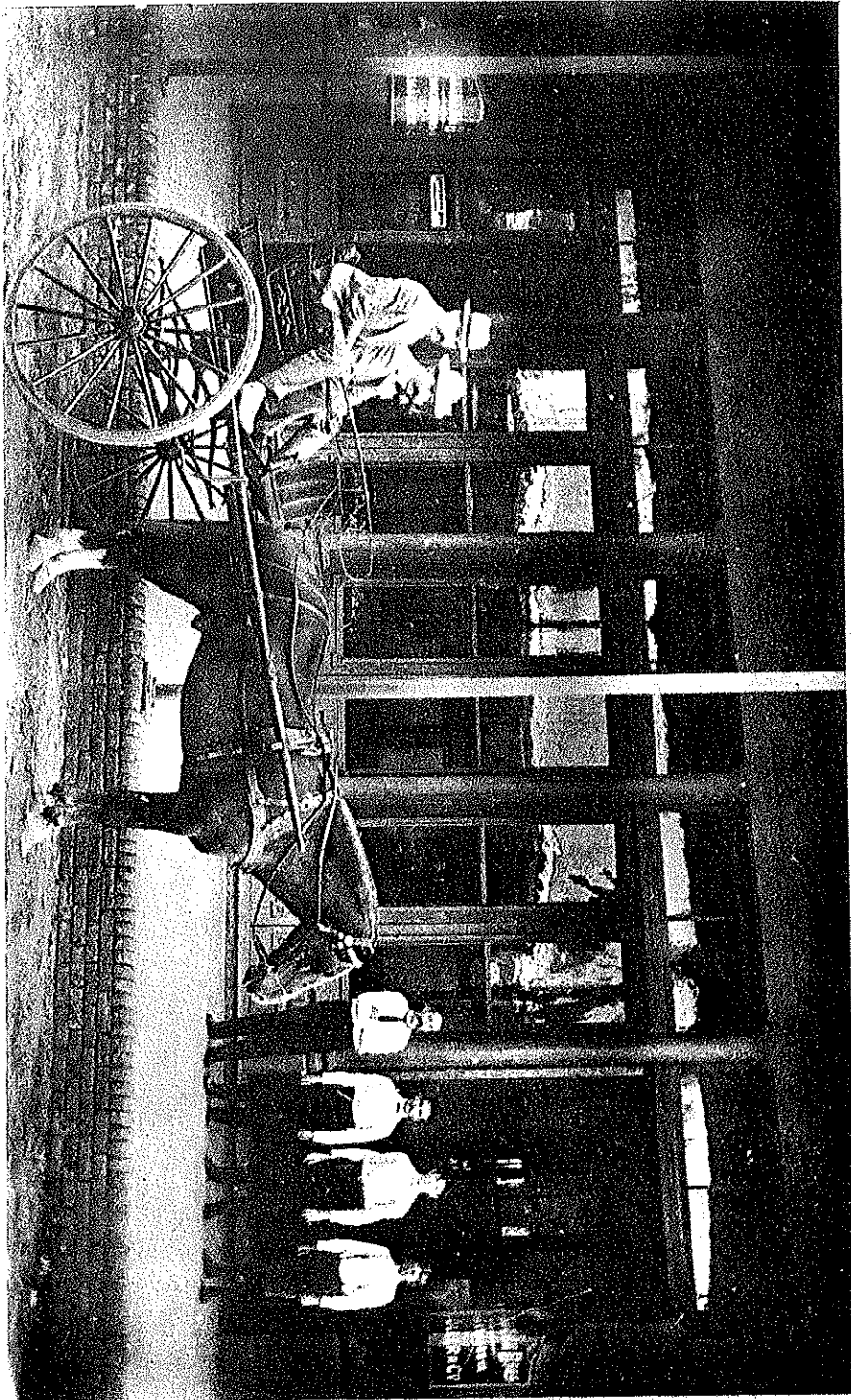


EXHIBIT 12 BRICK CURB AT ADOUE LOBIT BANK, STRAND AND 21ST, CIRCA 1906

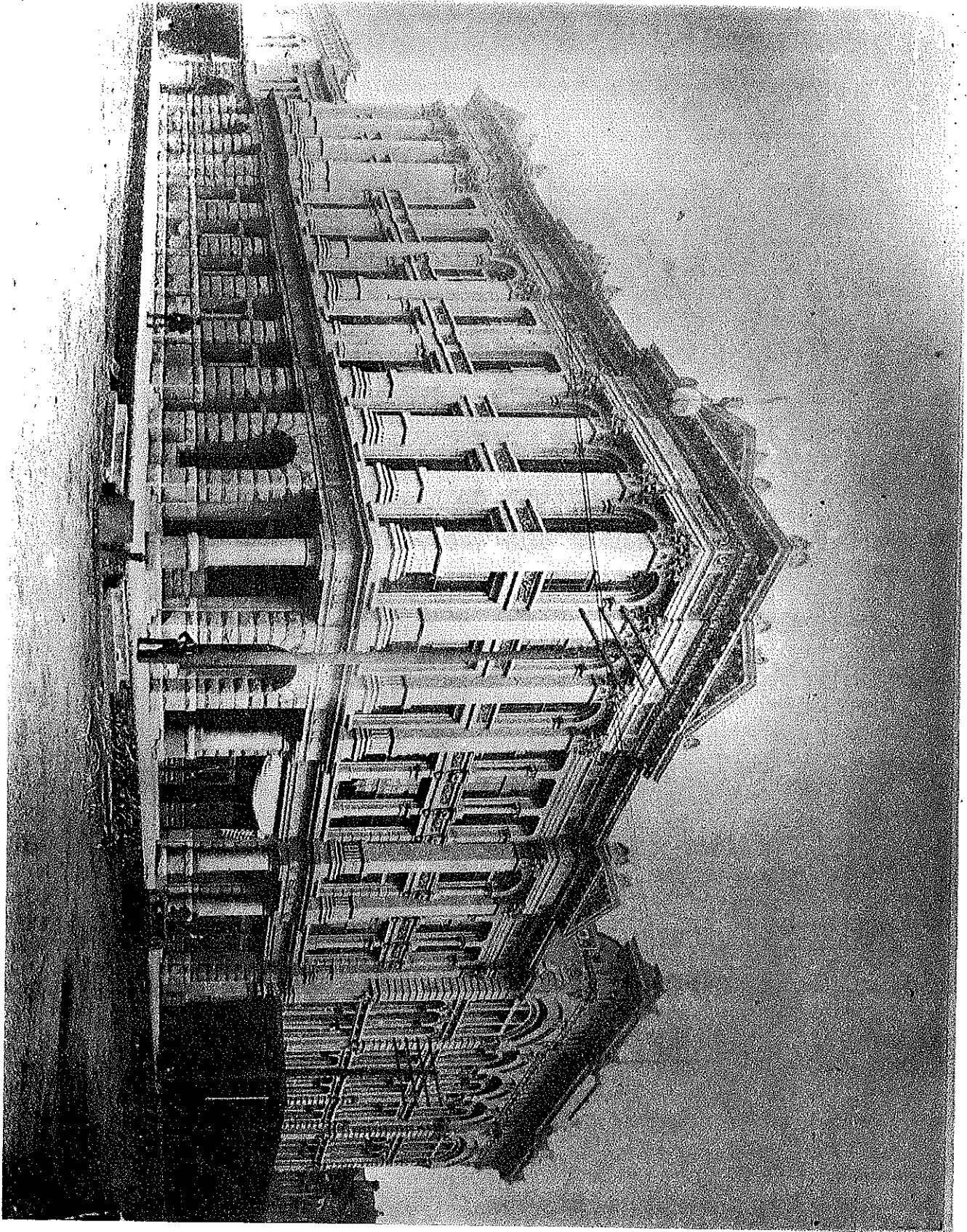


EXHIBIT 13: BRICK CURB AT HUTCHINGS SEALY BLDG., STRAND AND 24TH, CIRCA 1895

IX. Galveston Strand

LANDSCAPING

General Statements:

Historically, street type landscaping was not a part of the urban fabric of the Strand area in Galveston. Exhibits 14 through 17 (see p. 93) illustrate a density of use and character such that street landscaping probably could not have survived and also was a design refinement that had yet to be considered important to the area.

Galveston as a whole, however, did receive considerable landscaping. A number of plants and trees were located as being quite successful on the island beginning from about 1840. Even with an occasional hurricane in the last 120 years, the island today exhibits many types of hardy trees and plants. Broadway has excellent specimens of palms and live oaks. The new downtown also illustrates trees and plants acclimated to an urban environment.

The present day Strand presents a particular problem in terms of landscaping. It is reasonable to say that no landscaping existed therefore none should be required now. However, the 1880 Strand "streetscape" (see exhibits, p.46) of horses, carriages, barrels signs, telephones, people, and canopies has been lost in time to changing uses and destruction of many buildings. While a few blocks of buildings remain basically intact and although many fine buildings remain from 20th to 25th, the area suffers from a snaggle-tooth effect which has resulted in a loss of its original continuity.

Since historic buildings exist, in each block, it is important that continuity between them again be established. Proper street landscaping offers the potential for recreating this as well as providing an element which will help in the revitalization of the area.

Many alternatives exist for street landscape elements which would help reestablish continuity of the Strand. This study proposes that appropriate street landscape elements include street trees, plants and planting boxes placed from 20th

to 25th Streets.(see Drawings 18 thru 23 p. 86).. One general plan is for street trees to be placed along Strand at approximately every 25 feet, where vacant lots occur. Trees will be placed on both sides of the sidewalk to create canopy effect. Where existing or proposed canopies occur, planting boxes will be used for smaller plant materials (see drawing #15,p. 83). These planting boxes can also be used by individuals to define entries to buildings or provide a variety of color and texture on the sidewalks.

Recommendations:

Several alternatives exist for the placement of trees and other landscaping elements on The Strand. These are listed in order of preference.

Alternative I

Street trees would be installed approximately 25 feet on center from 20th to 25th.(see Drawings 18 thru 23, p. 86). Where vacant lots occur, trees would be placed on both sides of the sidewalk to create the canopy effect. Where existing or proposed canopies occur, planting boxes will be used for smaller plant materials and ornamental trees. (see drawing, p. 83). "Vest pocket" parks which can potentially occur at various points will also receive substantial landscaping with trees and plants being a different character and type than the street landscaping.

Alternative 2

Street trees would be planted at all gaps in the building line from 20th to 25th. Street trees in this alternative would be located (1) west of Hendley Row (2) west of the Commerce Building) (3) at the First Hutchings Sealy National Bank parking annex (4) at the Bock Meat Company lot and throughout much of the 2400 block.

Alternative 3

Street trees would be planted at all visually weak buildings. This alternative could work in combination with Alternative 2 (trees at gaps). This alternative assumes that the buildings listed do not have potential for facade restoration. Buildings where this planting would occur are (1) Marine Machine Works and adjacent building to the west (2) the Jules Lauve Building (3) the Bock Meat Co. (4) the Southern Supply Co. (5) hotel and storage shed on the south side of the 2400 block.

Alternative 4

Trees would be planted at off street parks only and canopies would be added to buildings that would be visually improved by the addition of canopies.

Alternative 5

Street trees and landscaping would be planted on a case by case basis with the previous alternatives utilized to determine the most successful planting system. If and when street trees and other landscaping elements are used on The Strand, the following tree types and plant types are recommended in order of preference.

Recommendations for Tree Types:

The basic tree for street planting should be Shumard Red Oak (first choice) or Live Oak (fast growing hybrid, second choice). This tree would be used near the street. For trees which form a second row behind the Oaks, or trees used in parks or courtyards, several types are recommended: the Chinese Tallow, the Honey Locus (Moraine Hybrid), Sweet Gum and Magnolia. See tree descriptions for some more information, p. 84 & 85.

Recommendations for Ground Covers and Secondary Planting:

The ground covers and the secondary plantings can be combined and can have flowering annuals mixed with them. The basic ground covers should be the Lairope, Jasmines (Chinese and Japanese), Santulina, and Monkey Grass. The Sweet Viburnum and Sandankwa Viburnum should be the basic shrubs. See p. 81. for descriptions and costs.

Cost for Street Planting:

Trees:

Live Oak	\$60.00 ea. - 3-½" caliper
	10.00 ea. - delivery
	<u>50.00</u> ea. - placement
	\$120.00 total/tree

Chinese Tallow, Honey Locust,

Sweet Gum and Magnolia

\$20.00 ea. - 1- $\frac{1}{2}$ "-3" caliper

3.00 ea. - delivery

50.00 ea. - placement

\$73.00 total/tree

Total Trees

110 Red Oak or Live Oak at \$120.00

\$14,200.00

33 Mix of three others at \$72.40

2,409.00

\$16,609.00

Total cost of trees and placement

\$16,609.00

COST OF ALTERNATE PROPOSALS:

Alternative I: Cost:

Concrete curb for tree grates - 3,408 lin. ft. @ \$2.50/lin. ft. = \$ 8,520.00

Tree grates - 143 @ \$123.00 ea. 17,589.00

Trees and placement 16,609.00

Approximate total cost \$42,718.00

Alternative II: Cost:

Brick curb - 3,408 lin. ft. @ \$2.50/lin ft. = \$ 8,520.00

Trees and placement = 16,609.00

Plants for planting bed

81 cans @ \$1.50 ea. - \$121.50/per bed

142 beds @ \$121.50 = 17,253.00

Approximate total cost \$42,382.00

Alternative III: Cost:

Concrete curb - 2,376 lin. ft. @ \$2.50/lin. ft.	= \$ 5,940.00
Tree grates - 100 @ \$123.00	12,300.00
Trees and placement	16,609.00
Brick curb - 1,032 sq. ft. @ \$2.50/sq. ft.	2,580.00
Plants for beds - 43 @ \$121.50 ea.	<u>5,224.50</u>
Approximate total cost	\$42,653.50

COST OF GROUND COVERS AND SECONDARY PLANTINGS:

Santulina - gray and green ground cover
\$1.50/gal. can + 40% for installation

Liriope - flowering ground cover
\$1.50/gal. can + 40% for installation

Star Jasmine - ground cover
\$0.25 ea. + 40% for installation

Monkey Grass - ground cover
\$1.50 gal. can + 40% for installation

Sweet Viburnum - shrub
\$9.00 ea., 24" high, installed

Sandankwa Viburnum - shrub
\$9.00 ea., 24" high installed

A general cost of \$10,000 should be used for ground covers and secondary plantings not used in conjunction with street trees.

Recommendation for a Park at Strand and 22nd:

Drawing 23 (p. 92) shows a proposed plan for a park at 22nd and Strand. This park would serve a variety of functions for Strand activity. It could be

used by the Galveston Art Center on The Strand for outside sculpture displays and evening gatherings. It could provide an overall gathering point for Strand festivals with a bandstand, tram stop and children's play sculpture area. It could provide an urban park atmosphere where people could eat a quiet lunch. It would serve as a model of successful landscapings in the Strand area as well as supplement to building the restoration activity.